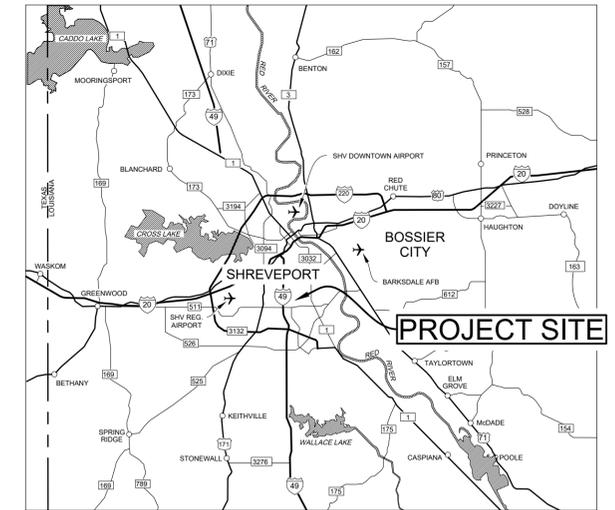


CITY OF SHREVEPORT
DEPARTMENT OF PUBLIC WORKS
CONSTRUCTION PLANS FOR
**SOUTHFIELD ROAD
IMPROVEMENTS**
PROJECT No. C25013
SHREVEPORT, LOUISIANA
FEBRUARY, 2026

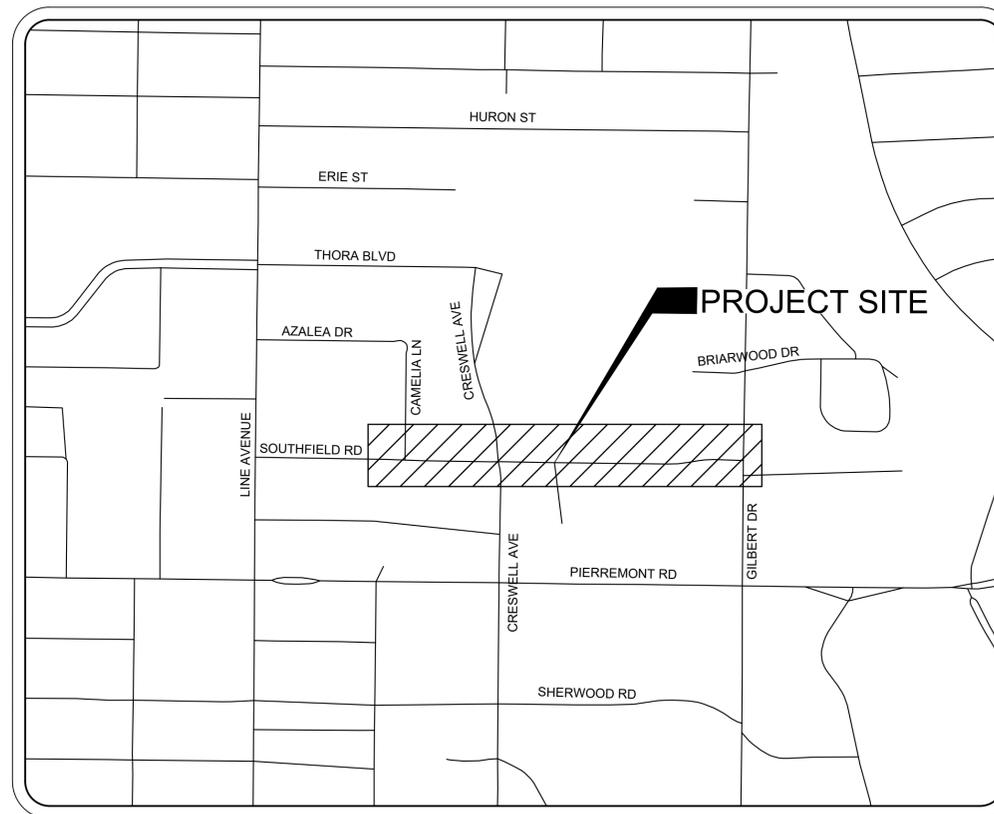


VICINITY MAP

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TOTAL SHEETS: 49



LAYOUT MAP

SCALE: 1" = 500'



ENGINEER

HALFF
401 MARKET STREET, SUITE 650
SHREVEPORT, LA 71101
CONTACT: JARED R. BOOGAERTS
EMAIL: JBOOGAERTS@HALFF.COM
TEL: 318-716-6136
FIRM/ BUSINESS NO.: 3072 STATE: LA

TYPE OF CONSTRUCTION

ROADWAY RECONSTRUCTION OF SOUTHFIELD ROAD FROM CAMELLIA LANE TO GILBERT DRIVE TO INCLUDE ADA COMPLIANT SIDEWALK, CURB AND GUTTER, SUBSURFACE DRAINAGE ON SOUTH SIDE OF THE ROAD, AND STREET LIGHTING.

DATUM

HORIZONTAL: NAD83 LA STATE PLANE/NORTH ZONE/US FOOT
VERTICAL: NAVD88

DESIGN SPEED

SOUTHFIELD ROAD: 25 MPH

NOTES:

1. CITY OF SHREVEPORT STANDARD SPECIFICATIONS FOR INFRASTRUCTURE IMPROVEMENTS, 2021 SHALL GOVERN THIS PROJECT.
2. PROJECT WAS DESIGNED WITH CURRENT ADA REQUIREMENTS.

MAYOR OF SHREVEPORT
TOM ARCENEUX

DISTRICT A REPRESENTATIVE TABATHA TAYLOR	DISTRICT B REPRESENTATIVE GARY BROOKS	DISTRICT C REPRESENTATIVE JIM TALIAFERRO
DISTRICT D REPRESENTATIVE GRAYSON BOUCHER	DISTRICT E REPRESENTATIVE DR. ALAN JACKSON, JR.	DISTRICT F REPRESENTATIVE JAMES GREEN
DISTRICT G REPRESENTATIVE URSULA BOWMAN	PUBLIC WORKS DIRECTOR JARVIS MORGAN	CITY ENGINEER DAVID B. SMITH, P.E.

OWNER/CLIENT:



505 TRAVIS STREET
SHREVEPORT, LA 71101
www.shreveportla.gov

PREPARED BY:



401 MARKET STREET, SUITE 650
SHREVEPORT, LA 71101
TEL. 318-716-6136
www.halff.com



2/17/2026



Know what's below.
Call before you dig.

THE INFORMATION SHOWN ON THESE DRAWINGS INDICATING SIZE, TYPE AND LOCATION OF UNDERGROUND, SURFACE, AND AERIAL UTILITIES IS NOT GUARANTEED TO BE EXACT OR COMPLETE. THE CONTRACTOR SHALL CONTACT THE "LOUISIANA 811 ONE CALL SYSTEM" AT 1-800-272-3020 48 HOURS PRIOR TO BEGINNING ANY EXCAVATION FOR EXISTING UTILITY LOCATIONS. THE CONTRACTOR SHALL ALSO BE FULLY RESPONSIBLE FOR FIELD VERIFYING LOCATIONS AND ELEVATIONS OF ALL EXISTING UTILITIES AFFECTED BY CONSTRUCTION FOR THIS PROJECT IN ORDER TO AVOID DAMAGING THOSE UTILITIES, AND SHALL IMMEDIATELY ARRANGE FOR REPAIR AND RESTORATION OF CONTRACTOR-DAMAGED UTILITIES TO THE UTILITY COMPANY'S APPROVAL AT THE EXPENSE OF THE CONTRACTOR.

GENERAL NOTES

- THE CONTRACTOR SHALL CALL LOUISIANA ONE CALL 1.800.272.3020. IT IS THE CONTRACTORS RESPONSIBILITY TO ADHERE TO THE REQUIREMENTS OF THE LOUISIANA ONE CALL NOTIFICATION PROGRAM. LOUISIANA STATE LAW R.S. 40:1749.13 REQUIRES EXCAVATORS AND DEMOLISHERS TO SERVE TELEPHONIC NOTICE OF THE INTENT TO EXCAVATE OR DEMOLISH TO A REGIONAL NOTIFICATION CENTER. THIS NOTICE SHALL BE GIVEN AT LEAST 48 HOURS, BUT NOT MORE THAN 120 HOURS, EXCLUDING WEEKENDS AND HOLIDAYS, IN ADVANCE OF THE COMMENCEMENT OF ANY EXCAVATION OR DEMOLITION ACTIVITY.
- CONTRACTOR SHALL REMOVE, SALVAGE, AND REPLACE MAILBOXES, MASONRY COLUMNS, AND BRICK PLANTERS AS REQUIRED FOR CONSTRUCTION. MAILBOXES SHALL BE REPLACED 12 INCHES FROM THE BACK OF CURB, WITH THE MAILBOX HEIGHT MATCHING THE EXISTING CONDITIONS. MAIL DELIVERY SHALL BE UNINTERRUPTED DURING ALL PHASES OF CONSTRUCTION. MASONRY PAVERS DAMAGED OR UNUSABLE FOR RECONSTRUCTING MAILBOXES, COLUMNS, OR PLANTERS SHALL BE REPLACED WITH MASONRY PAVERS OF SIMILAR SIZE, COLOR, TEXTURE, AND MATERIAL.
- CONTRACTOR SHALL BE RESPONSIBLE FOR TAKING ALL PRECAUTIONS TO PROTECT EXISTING TREES, SHRUBBERY, AND OTHER LANDSCAPING AS MUCH AS POSSIBLE WITHIN AND ADJACENT TO THE THE LIMITS OF CONSTRUCTION. ANY LANDSCAPING DISTURBED OR DAMAGED BY CONSTRUCTION ACTIVITIES SHALL BE REPLACED BY THE CONTRACTOR AT NO DIRECT PAY.
- CONTRACTOR SHALL LOCATE, VERIFY WORKING CONDITIONS, AND PROTECT ALL EXISTING IRRIGATION SYSTEMS' LINES AND HEADS (IF ANY). REMOVE, ADJUST AND REINSTALL IN GOOD CONDITION EQUAL TO OR BETTER THAN EXISTING CONDITION; REPLACE, IF IN DIRECT CONFLICT, WITH THE SAME OR BETTER QUALITY MATERIAL AND APPURTENANCES.
- CONSTRUCTION METHODS AND MATERIALS SHALL BE IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS AS SUPPLEMENTED AND/OR AMENDED BY THE PLANS, SUPPLEMENTAL SPECIFICATIONS, AND OTHER SPECIAL PROVISIONS, DATED 2/3/2026.
- CONTRACTOR SHALL CONTACT THE CITY OF SHREVEPORT REPRESENTATIVE PRIOR TO CONSTRUCTION TO COORDINATE WORK.
- CAUTION!!! ALL EXISTING UTILITIES PLOTTED FROM INFORMATION PROVIDED BY THE RESPECTIVE UTILITY COMPANIES, AVAILABLE AS-BUILT DRAWINGS, AND FIELD LOCATION OF ABOVE GROUND FEATURES ONLY. NOT ALL EXISTING UTILITIES MAY BE SHOWN ON THE DRAWINGS. THE ENGINEER AND CITY ASSUME NO LIABILITY FOR DAMAGES TO EXISTING UTILITIES NOT SHOWN ON THE PLANS, OR DAMAGES RESULTING FROM DISCREPANCIES BETWEEN THE LOCATION OF UTILITIES SHOWN ON THE PLANS, AND THEIR ACTUAL LOCATION.
- CONTRACTOR SHALL VERIFY ALIGNMENT AND GRADE OF ALL PROPOSED IMPROVEMENTS PRIOR TO CONSTRUCTION. ANY DISCREPANCIES AND/OR CONFLICTS THAT ARE DISCOVERED SHALL BE REPORTED TO THE PROJECT ENGINEER FAR ENOUGH IN ADVANCE TO MAKE ANY REQUIRED ADJUSTMENTS TO THE DESIGN.
- IT IS CONTRACTOR'S RESPONSIBILITY TO PROVIDE FOR SAFE ACCESS (INGRESS AND EGRESS) IN ALL WEATHER CONDITIONS TO PROPERTY OWNERS AND TO VEHICULAR TRAFFIC DURING THE CONSTRUCTION PERIOD. THE CONTRACTOR SHALL STRICTLY ADHERE TO CITY, STATE AND FEDERAL REQUIREMENTS FOR SAFETY DURING CONSTRUCTION, AND SHALL REVISE AND UPDATE THE TRAFFIC CONTROL & SEQUENCING PLANS MONTHLY OR AS NECESSARY, AND AS APPROVED BY CITY AND PROJECT ENGINEER.
- THE CONTRACTOR SHALL PROVIDE PROPER BARRICADES AND SIGNAGE, AND MAINTAIN TRAFFIC FLOW IN ACCORDANCE WITH THE M.U.T.C.D. AND ANY OTHER APPLICABLE REGULATIONS.
- WHERE DEMOLITION OF CONCRETE AND/OR ASPHALT PAVEMENT, CONCRETE SIDEWALKS, CONCRETE CURBS, ETC. IS REQUIRED FOR CONSTRUCTION, CONTRACTOR SHALL SAW-CUT PRIOR TO DEMOLITION TO PROVIDE A NEAT EDGE FOR TIE TO NEW CONSTRUCTION, OR CONTRACTOR SHALL REMOVE TO NEAREST JOINT. RESTORE DEMOLISHED UNPAVED AREAS TO PRE-CONSTRUCTION CONDITIONS AT NO DIRECT PAY.
- CONTRACTOR IS RESPONSIBLE FOR MAINTAINING SAFE CONDITIONS ON ADJACENT ROADWAYS AND ROAD SHOULDERS, AND SHALL TAKE IMMEDIATE ACTION TO REMOVE DEBRIS, MUD, EXCESS CONSTRUCTION WATER, AND ANY OTHER UNSAFE ITEMS FROM THE AREA. FAILURE TO COMPLY MAY RESULT IN THE CITY CLEANING UP AND THE RESULTING COSTS BEING SUBTRACTED FROM THE CONTRACT.
- EXISTING TRAFFIC SIGNS TO REMAIN IN PLACE DURING CONSTRUCTION, IF POSSIBLE. SIGNS REMOVED AND RELOCATED OR REPLACED TO BE IN EQUAL OR BETTER CONDITION.
- ALL EXCAVATIONS SHALL BE PERFORMED IN STRICT ACCORDANCE WITH OSHA REGULATIONS AND ANY OTHER APPLICABLE LOCAL, STATE, AND FEDERAL CODES AND ORDINANCES.
- DURING CONSTRUCTION, POSITIVE DRAINAGE SHALL BE MAINTAINED IN ALL DRAINAGE DITCHES AND CHANNELS.
- ALL EARTHWORK, SOIL MATERIALS, AND PAVEMENT REQUIREMENTS INCLUDING BUT NOT LIMITED TO SUBGRADE PREPARATION, COMPACTION, LIME TREATMENT, FILL CONSTRUCTION, CONCRETE PAVEMENT DESIGN, FLEXIBLE PAVEMENT DESIGN, JOINT SPACING, AND PREPARATION SHALL ADHERE TO ALL CITY REQUIREMENTS FOR RESIDENTIAL AND COMMERCIAL STREET PAVEMENT, AND TO GEOTECHNICAL REPORT "PROJECT DRGeoES 24103G", PERFORMED BY DAVE RAMBARAN, DATED DECEMBER 09, 2024, AND INCLUDED IN PROJECT MANUAL FOR SOUTHFIELD RD IMPROVEMENTS.
- CONTRACTOR IS NOT PERMITTED TO PARK EQUIPMENT OR VEHICLES ON GRASS. CONTRACTOR AND WORKERS MUST PARK VEHICLES AND EQUIPMENT ON ROADWAY OR ADJACENT STREETS. ANY RUTTING, OR DAMAGE TO YARDS CAUSED BY CONTRACTOR'S EQUIPMENT OR WORKER'S VEHICLES WILL BE FIXED AT THE CONTRACTOR'S EXPENSE.
- ALL WORK IS SUBJECT TO FINAL INSPECTION BY THE CITY AND PROJECT ENGINEER.
- CONTRACTOR SHALL COOPERATE WITH THE VARIOUS PARTIES INVOLVED IN THE DELIVERY OF MAIL, SCHOOL BUS ROUTES AND THE COLLECTION AND REMOVAL OF TRASH AND GARBAGE TO MAINTAIN EXISTING SCHEDULES FOR THESE SERVICES.
- CONTRACTOR SHALL ENSURE EMS SERVICES HAVE ACCESS TO ALL RESIDENTS DURING CONSTRUCTION.
- ALL STREET LIGHTS SHALL HAVE HOUSE SIDE SHIELDING INSTALLED TO PREVENT LIGHT SHED ON HOMES. REFER TO LIGHT FIXTURE SCHEDULE DESCRIPTION ON SHEET E001.

PROJECT BIDDING NOTES

- THE CONTRACTOR SHALL THOROUGHLY REVIEW THE PLANS AND SPECIFICATIONS, PREPARE DETAILED MATERIAL TAKE-OFFS, MAKE SITE VISITS AS REQUIRED, AND ESTIMATE THEIR LABOR AND MATERIAL QUANTITIES AND COSTS ACCORDINGLY BEFORE SUBMITTING THEIR BID. THIS WORK SHALL BE BID ON UNIT PRICE BASIS. ANY WORK, LABOR, MATERIALS, EQUIPMENT, FITTINGS, AND/OR APPURTENANCES REQUIRED BY THE PLANS AND SPECIFICATIONS FOR COMPLETION OF THE PROJECT, AND NOT OTHERWISE IDENTIFIED SHALL BE DEEMED INCIDENTAL TO CONSTRUCTION AND SHALL BE PERFORMED, SUPPLIED AND INSTALLED AT NO ADDITIONAL PAY.

EROSION CONTROL GENERAL NOTES

- CONTRACTOR SHALL COMPLY WITH ALL FEDERAL, STATE, AND LOCAL STORMWATER REGULATIONS. CONTRACTOR SHALL ACQUIRE ALL REQUIRED PERMITS PRIOR TO COMMENCEMENT OF WORK. SEE SPECIAL CONDITIONS SECTION OF PROJECT MANUAL FOR ADDITIONAL CONTRACTOR REQUIREMENTS.
- CONTRACTOR SHALL CONFORM ACTIVITIES TO THE SWPPP AS SPECIFIED, INCLUDING INSTALLING, MAINTAINING, AND REMOVING POLLUTION CONTROLS, CONDUCTING AND DOCUMENTING INSPECTIONS OF POLLUTION CONTROLS, SPRINKLING FOR DUST CONTROL, MAINTAINING SPILL RESPONSE EQUIPMENT ON-SITE, AND "GOOD HOUSEKEEPING". POLLUTION CONTROLS INCLUDE SILT FENCES, STABILIZED CONSTRUCTION ENTRANCE, ESTABLISHING GRASS, AND SPRINKLING FOR DUST CONTROL.
- APPROXIMATELY 1.78 ACRES OR .38 MILES OF ROADWAY WILL BE DISTURBED WITH THIS CONSTRUCTION.
- CONTRACTOR SHALL CLEAN ALL NEW STORM DRAIN LINES, BOX CULVERTS, AND CHANNELS AFTER COMPLETION OF CONSTRUCTION. SAID FACILITIES SHALL BE MAINTAINED UNTIL STABILIZATION OF DISTURBED AREAS IS COMPLETE.

TEMPORARY TRAFFIC CONTROL

- TEMPORARY WORK ZONE TRAFFIC CONTROL SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 1306 OF THE CITY OF SHREVEPORT STANDARD SPECIFICATIONS, THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.), AND THE TRAFFIC CONTROL STANDARD PLANS INCLUDED IN THE PLAN SET (WHERE APPLICABLE). ANY ADDITIONAL SIGNAGE OR OTHER DEVICES REQUIRED BUT NOT SHOWN ON THE SUGGESTED SEQUENCE OF CONSTRUCTION AND MINIMUM CONSTRUCTION SIGNING SHEETS INCLUDED IN THE PLAN SET SHALL BE PROVIDED AT NO ADDITIONAL PAY.
- CONTRACTOR SHALL MAKE ADJUSTMENTS TO PLAN (FOR CONSTRUCTION SEQUENCING AND TEMPORARY SIGNS AND BARRICADES) IF REQUIRED TO ACCOMMODATE TURN MOVEMENTS FOR OVERSIZE VEHICULAR TRAFFIC OR OTHER UNANTICIPATED SITUATIONS AT INTERSECTIONS.

CURB RAMPS:

- MAXIMUM ALLOWABLE CROSS SLOPE OF CURB RAMP SURFACES IS 2%; DESIRED CROSS SLOPE IS 1.5%.
- GRADE BREAKS AT THE TOP AND BOTTOM OF CURB RAMPS RUNS SHALL BE PERPENDICULAR TO THE DIRECTION OF THE RAMP RUN.
- WHERE CURB RAMPS ARE LOCATED ADJACENT TO A WALKING SURFACE, A FLARE MUST BE PROVIDED; OTHERWISE, A CURB MAY BE PROVIDED.
- THE LANDING DIMENSIONS ARE 5'X5' WITH A MAXIMUM 2% SLOPE IN ANY DIRECTION. THE LANDING CLEAR WIDTH SHALL BE AT LEAST AS WIDE AS THE WIDEST RAMP RUN LEADING TO THE LANDING.
- DRAINAGE STRUCTURES SHOULD BE LOCATED ON THE UPSTREAM SIDE OF THE RAMP AND LOCATED TO PREVENT PONDING NEAR THE CURB RAMP. DRAINAGE STRUCTURES SHOULD BE PLACED OUTSIDE THE CROSSWALK.
- SLOPES OF ADJOINING GUTTERS AND ROADWAY SURFACES IMMEDIATELY ADJACENT TO THE CURB RAMP SHALL NOT EXCEED 5%.
- CURB RAMPS SHOULD BE ALIGNED WITH THE DIRECTION OF PEDESTRIAN TRAVEL ON THE CROSSWALK OR THEORETICAL CROSSWALK.
- CURB RAMPS SHALL INCLUDE DETECTABLE WARNING SURFACES WITH TRUNCATED DOMES.
- WHERE A CURB RAMP IS CONSTRUCTED WITHIN EXISTING CURB, CURB AND GUTTER AND/OR SIDEWALK, THE EXISTING CURB OR CURB AND GUTTER SHALL BE REMOVED TO THE NEAREST JOINT BEYOND THE CURB TRANSITION OR THE EXTENT THAT NO REMAINING SECTION OF CURB OR CURB AND GUTTER IS LESS THAN 5' LONG OR AS DIRECTED BY THE PROJECT ENGINEER.
- EXISTING SIDEWALKS SHALL BE REMOVED TO THE NEAREST JOINT BEYOND THE FLARE SLOPE OR TO THE EXTENT THAT NO REMAINING SECTION OF SIDEWALK IS LESS THAN 5' LONG OR AS DIRECTED BY THE PROJECT ENGINEER.
- ALL SLOPES SHOWN ARE MAXIMUM ALLOWABLE. THE LEAST POSSIBLE SLOPE THAT WILL DRAIN PROPERLY WILL BE USED.
- CURB RAMPS SHALL BE PLACED AND DESIGNED WHERE PONDING DOES NOT OCCUR AT THE BOTTOM OR ON THE CURB RAMP.
- CURB RAMPS PLACED OUTSIDE THE RADIUS OF THE CURB ARE PREFERRED. PAIRED CURB RAMPS PLACED WITHIN THE RADIUS OF A CURB ARE ACCEPTABLE. SINGLE CURB RAMPS PLACED ON THE APEX OF A CURB SHOULD NOT BE USED UNLESS SITE CONSTRAINTS, SUCH AS THE LOCATION OF DRAINAGE STRUCTURES, REQUIRE IT.

DETECTABLE WARNING SURFACES:

- FOR ADA COMPLIANCE, DETECTABLE WARNING SURFACES MUST BE PROVIDED ON ALL PEDESTRIAN CURB RAMPS, MEDIANS AND PEDESTRIAN REFUGE ISLANDS (WIDTH 6' OR GREATER), RAILROAD CROSSINGS AND AT-GRADE SIDEWALK AND SHARED-USE PATHS INTERSECTING WITH ROADWAYS.
- CURB RAMPS MUST CONTAIN A DETECTABLE WARNING SURFACE THAT CONSISTS OF RAISED TRUNCATED DOMES COMPLYING WITH ADA GUIDELINES. THE SURFACE MUST CONTRAST VISUALLY WITH ADJOINING SURFACES. COLOR FOR DETECTABLE WARNING SURFACE SHALL CONTRAST VISUALLY WITH ADJOINING SURFACES, EITHER LIGHT-ON-DARK OR DARK-ON-LIGHT.
- DETECTABLE WARNING SURFACES MUST BE SLIP RESISTANT AND NOT ALLOW WATER TO ACCUMULATE.
- TRUNCATED DOMES SHOULD BE ALIGNED PERPENDICULAR OR RADIAL TO THE GRADE BREAK BETWEEN THE CURB RAMP OR AT-GRADE SIDEWALK AND THE STREET.
- DETECTABLE WARNING SURFACES SHALL BE A MINIMUM OF 24" IN DEPTH IN DIRECTION OF PEDESTRIAN TRAVEL AND EXTEND THE FULL WIDTH OF THE RAMP RUN OR LANDING WHERE THE PEDESTRIAN ACCESS ROUTE ENTERS THE STREET. SOME DETECTABLE WARNING PRODUCTS MAY REQUIRE A CONCRETE BORDER. THE CONCRETE BORDER SHOULD NOT EXCEED 2".
- DETECTABLE WARNING SURFACES SHALL BE PLACED AT THE BACK OF CURB OR NO GREATER THAN 5' FROM THE BACK OF CURB. DETECTABLE WARNING SURFACES MAY BE CURVED ALONG THE CORNER RADIUS.

FILE NAME: A:\47000\47786\038\PW\CADD\Sheets\C-PLAN-G002-GN1S-47786-038.dwg DATE: February 17, 2026, TIME: 5:01 PM, USER: ah5754

NO.	DATE	DESCRIPTION

JG
DESIGNED BY:
JG
DRAWN BY:
JB
CHECKED BY:



2/17/2026



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.:	47786.038
ISSUED:	2/2/26
SCALE:	NONE
SHEET TITLE	GENERAL NOTES
	C2.0

C-SHEETS

ITEM NUMBER	DESCRIPTION	QTY.	UNIT
302.1.1	REMOVAL OF STRUCTURES AND OBSTRUCTIONS (EXISTING STORM PIPE)	931	LF
302.1.2	REMOVAL OF STRUCTURES AND OBSTRUCTIONS (EXISTING WATER MAIN)	242	LF
302.1.3	REMOVAL OF STRUCTURES AND OBSTRUCTIONS (EXISTING CATCH BASINS)	5	EA
302.7.1	REMOVAL OF PAVEMENT BASE & SURFACE (ASPHALT PAVEMENT)	4780	SY
302.7.2	REMOVAL OF PAVEMENT BASE & SURFACE (CONCRETE PAVEMENT)	323	SY
302.8	REMOVAL OF CONCRETE WALKS AND DRIVEWAYS (INCLUDES BASE)	543	SY
303.1	ROADWAY EXCAVATION	2548	CY
310.2	TEMPORARY HAY OR STRAW BALES	12	EA
403.5.1	CRUSHED AGGREGATE BASE COURSE (8" THICK) (ROADWAY BASE)	6636	SY
403.6	SAND BASE COURSE	299	CY
507.3.1	ASPHALTIC CONCRETE (BINDER COURSE) (1.5" THICK)	4668	SY
507.3.2	ASPHALTIC CONCRETE (WEARING COURSE) (1.5" THICK)	4668	SY
509.1	PORTLAND CEMENT CONCRETE PAVEMENT (8" THICK)	327	SY
601.5	SINGLE CURB INLET (5' CAST-IN-PLACE)	13	EA
601.10.1	3' X 3' CONCRETE JUNCTION BOX	2	EA
601.10.2	4' X 4' CONCRETE JUNCTION BOX	1	EA
605.1.1	CONCRETE CURB & GUTTER (MOUNTABLE)	3624	LF
605.1.2	CONCRETE CURB & GUTTER (BARRIER)	322	LF
605.4	SIDEWALKS (4" THICK)	664	SY
605.5.1	DRIVEWAYS (6" THICK)(IN KIND)(WASHED GRAVEL)	24	SY
605.5.2	DRIVEWAYS (6" THICK)(CONCRETE)	392	SY
605.6	HANDICAP RAMPS (ALL TYPES)	3	EA
1001.1.1	PIPE LINE IN PLACE (12" HDPE)	9	LF
1001.1.2	PIPE LINE IN PLACE (15" CPPPDW)	618	LF
1001.1.3	PIPE LINE IN PLACE (18" CPPPDW)	557	LF
1201.8	SOD LAWN (MATCH NATIVE TYPE)	1633	SY
1202.1	MOBILIZATION	1	LS
1301.21	U-CHANNEL POST	3	EA
1306.1	TEMPORARY SIGNS AND BARRICADES	1	LS
2210.3	REBUILD CHIMNEY/REPLACE CAST IRON FRAME AND COVER	2	EA
3000.1	WATER MAIN (6") (PVC)	244	LF
3100.1	TESTING, FLUSHING, AND STERILIZATION OF WATER MAINS	1	LS
3300.1	RELOCATE FIRE HYDRANT	1	EA
3400.2	RELOCATE BUTTERFLY VALVE	1	EA
S-1	MAILBOXES (METAL) (RELOCATE)	13	EA
S-2	MAILBOXES (BRICK) (RECONSTRUCT SAME MATERIAL)	3	EA
S-3	MAILBOXES (PLASTIC) (RELOCATE)	3	EA
S-4	PROOF ROLLING EXISTING SUBGRADE	1	LS
S-5	RECONNECT EXISTING YARD DRAINS	1	LS
S-6	SAWCUTTING	735	LF
S-7	AS-BUILTS	1	LS
S-8	CONSTRUCTION LAYOUT	1	LS
S-9	NYLOPLAST DRAIN BASIN (12")	1	EA
S-10	BI-AXIAL GEOGRID	6636	SY

E-SHEETS

ITEM NUMBER	DESCRIPTION	QTY.	UNIT
1302 (1)	TRENCHING & BACKFILLING (1 1/2" C)	1880	LF
S-011	SCHEDULE 40 PVC DUCT - 1 1/2" C	1880	LF
S-012	600V CABLE - # 1 AWG	40	LF
S-013	600V CABLE - # 8 AWG	20	LF
S-014	600V CABLE - # 10 AWG	5640	LF
S-015	GROUND RODS - 5/8" X 8'	1	EA
S-016	PEDESTAL ENCLOSURE	1	EA
S-017	PEDESTAL	1	EA
S-018	PEDESTAL BASE	1	EA
S-019	MAIN CIRCUIT BREAKER - 100A	1	EA
S-020	SURGE PROTECTION DEVICE	1	EA
S-021	LIGHTING CONTACTOR - 30A/4P	1	EA
S-022	BRANCH CIRCUIT BREAKER - 15A	2	EA
S-023	INTERIOR PANELS	1	EA
S-024	DECORATIVE CONCRETE POLE AND FIXTURE	10	EA
S-028	LIGHT POLE FOUNDATION	10	EA
S-029	MISCELLANEOUS CONSTRUCTION ITEMS	1	LS

FILE NAME: A:\47000s\47786\038\PC\CADD\Sheets\C-PLAN-G003-QUAN-47786.038.dwg DATE: February 18, 2026, TIME: 8:48 AM, USER: ah5754

REVISION NO.	DATE	DESCRIPTION

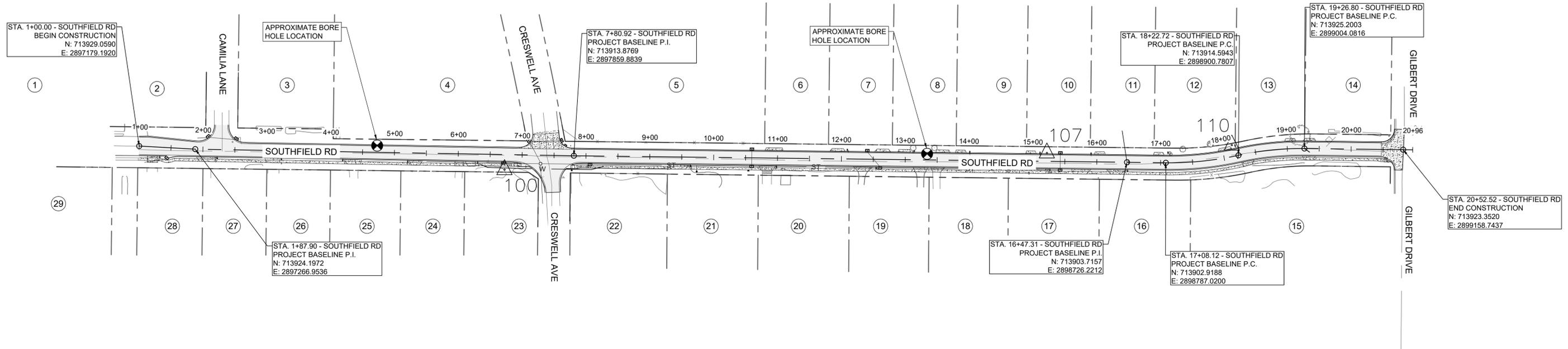
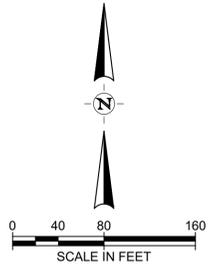
DESIGNED BY: JB
 DRAWN BY: JG
 CHECKED BY: JB



**SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA**

PROJECT NO.: 47786.038
 ISSUED: 9/30/25
 SCALE: NONE
 SHEET TITLE: SUMMARY OF QUANTITIES
C3.0

FILE NAME: A:\47000\47786\038\PW\CADD\Sheet\C-PLAN-G004-PROJ-47786-038.dwg DATE: February 17, 2026, TIME: 5:01 PM, USER: ah5764



⊗ PARCEL OWNER INFORMATION TABLE

1	STONERIDGE AUTO, LLC	16	GOODMAN, CARL GERALD AND
2	JOHNSON, JESSE AND	17	MCINNIS, KYLE C AND
3	BROYLES REBECCA ALICE AND	18	HENDERSON, ELIZABETH HEUER
4	CADENCE BANK SUCCESSOR TRUSTEE (OF THE R.	19	MILSTEAD, MARION EZRA &
5	CADENCE BANK SUCCESSOR TRUSTEE (OF THE R.	20	MOSS, JARED AND
6	JELKS, BOBBY EARL AND	21	JOHNSON, MARSHALL WILLIAM AND
7	KISTLER, ERNEST L., III	22	SELLA, JOHN W
8	FRANKS, ROBERT DEAN JR. AND VICTORIA NEWPORT	23	STOKES, RONNIE EUGENE AND ANN MC CRAW STOKES
9	OWEN, KIM THOMAS	24	CHILDS, WILLIAM ALVIN, JR.
10	WHITE, PAUL STEPHEN AND	25	DUNN, ADRIAN JOHN &
11	DURHAM, II, EDWARD AND KAREN BURNS DURHAM	26	BAKER, JR., STEPHEN D. AND
12	FOLSOM, CHARLES HAGEN	27	PATTON, STANLEY MEADE
13	BUSADA, MITCHELL GEORGE AND	28	HARRIS, JAMEY WAYNE AND
14	CHOPIN, EDWARD JOSEPH	29	SOUTHFIELD PROPERTIES, L.L.C.
15	BAKOWSKI, GEORGE MICHAEL AND		

CONTROL POINT TABLE

POINT#	ELEVATION	NORTHING	EASTING	DESCRIPTION
100	201.929	713892.1360	2897751.3740	SET 1/2"IP
107	218.31	713918.1930	2898600.7510	60 DMAG
110	226.28	713930.5070	2898890.3710	60D NAIL

LEGEND:

- PROPERTY BOUNDARY
- PROPOSED ROADWAY GEOMETRY
- ⊗ PARCEL NUMBER
- △ TEMPORARY BENCHMARK
- ⊗ BOREHOLE

NOTES:

- EXISTING SURVEY PER RED CHUTE SURVEYING, LLC. JASON WESTERMAN PLS (LA. REG. NO. 5203)
- HORIZONTAL CONTROL USED FOR THIS PROJECT IS NAD83 LOUISIANA STATE PLANE, NORTH ZONE.
- VERTICAL CONTROL USED FOR THIS PROJECT IS FROM THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
- CONTRACTOR SHALL GIVE 7 CALENDAR DAYS NOTICE TO EACH PROPERTY OWNER, ASSOCIATION, OR GROUP PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES VIA WRITTEN LETTER OR PUBLIC NOTICE FOR EACH CONSTRUCTION PHASE.

REVISION NO.	DATE	DESCRIPTION

JG
DESIGNED BY:
JG
DRAWN BY:
JB
CHECKED BY:



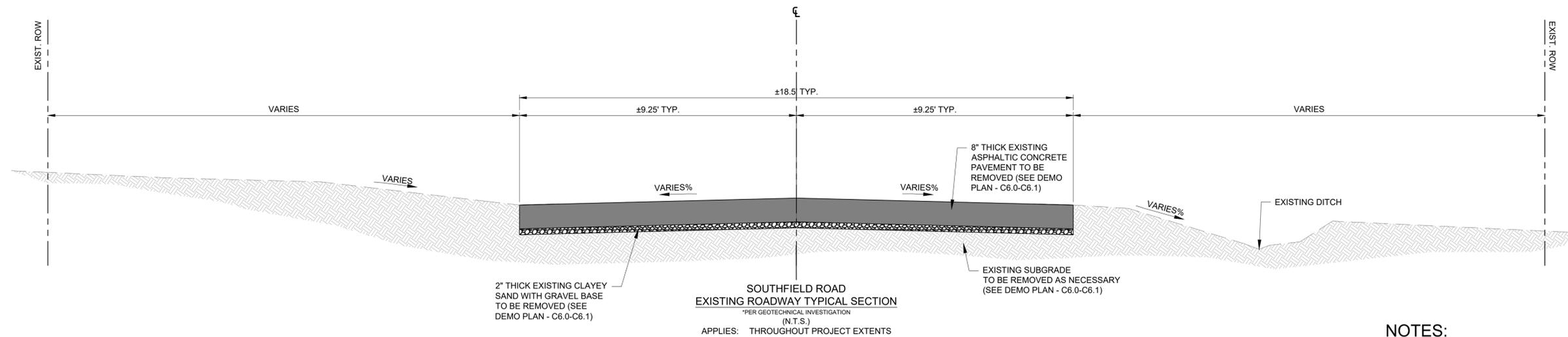
2/17/2026



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

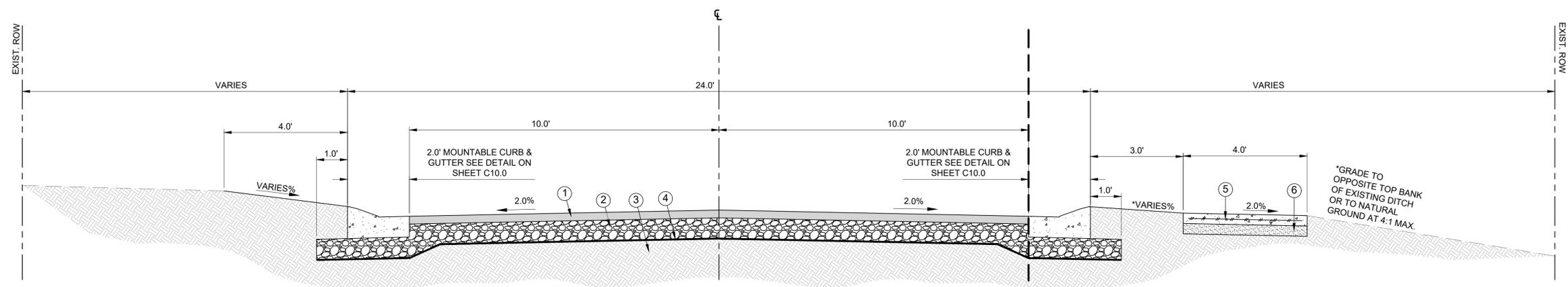
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ISSUED: 9/30/2025
SCALE: 1" = 80'
SHEET TITLE
PROJECT LAYOUT

C4.0

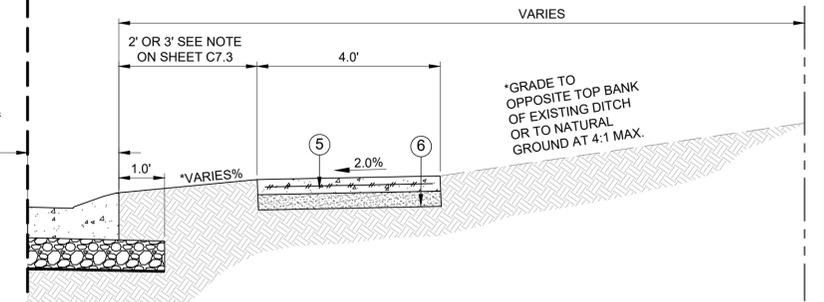


**SOUTHFIELD ROAD
EXISTING ROADWAY TYPICAL SECTION**
(N.T.S.)
APPLIES: THROUGHOUT PROJECT EXTENTS

NOTES:
1. SEE APPENDIX A OF SPECIFICATIONS FOR GEOTECHNICAL REPORT



**SOUTHFIELD ROAD
PROPOSED ROADWAY TYPICAL SECTION A
(SIDEWALK SLOPING AWAY FROM ROAD)**
(N.T.S.)
APPLIES: STA. 1+00.00 - STA. 11+00.00
STA. 12+60.72 - STA. 17+96.65



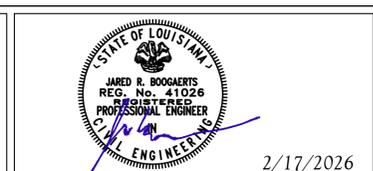
**SOUTHFIELD ROAD
PROPOSED PARTIAL TYPICAL SECTION B
(SIDEWALK SLOPING TOWARDS ROAD)**
(N.T.S.)
APPLIES: STA. 11+00.00 - STA. 12+60.72
STA. 17+96.65 - STA. 20+41.60

- LEGEND**
- ① 3" THICK ASPHALTIC CONCRETE (1.5" OF BINDER COURSE AND 1.5" OF WEARING COURSE)
 - ② 8" THICK AGGREGATE BASE COMPACTED TO 95% STD PROCTOR.
 - ③ SCARIFY AND COMPACT MINIMUM 1' OF EXISTING SUBGRADE TO 95% STD PROCTOR
 - ④ BI-AXIAL GEOGRID
 - ⑤ 4" THICK PORTLAND CEMENT CONCRETE SIDEWALK W/ 6"x6" W1.04/W1.4 @ 2" MIN COVER
 - ⑥ 4" THICK SAND BASE

FILE NAME: A:\47000\47786\038\PIV\CADD\Sheets\C-PLAN-C100-TYPES-47786-038.dwg DATE: February 17, 2026, TIME: 5:01 PM, USER: ah5754

NO.	DATE	DESCRIPTION

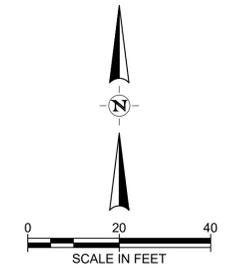
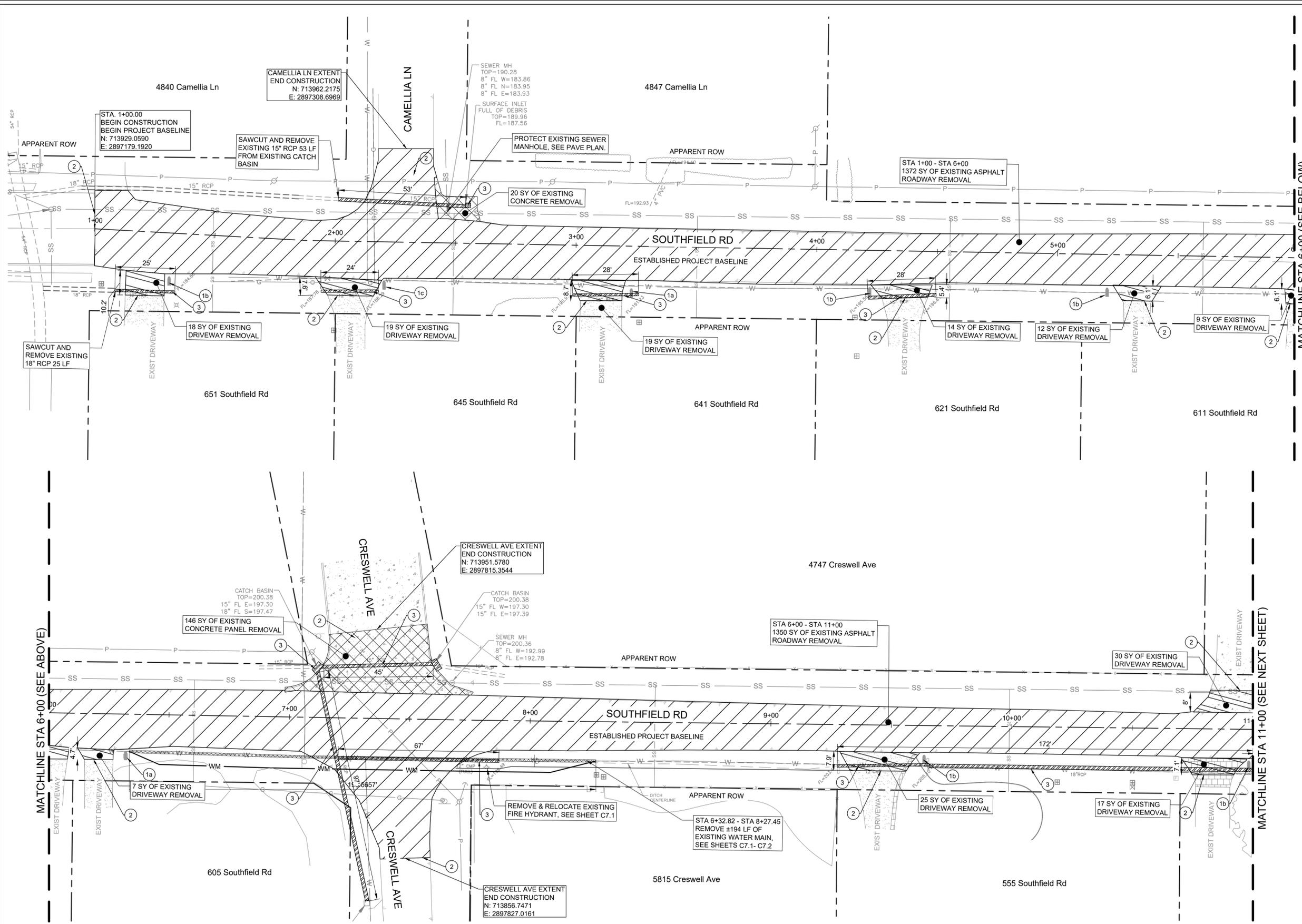
DESIGNED BY: JG
DRAWN BY: JB
CHECKED BY: JB



**SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA**

PROJECT NO.: 47786.038
ISSUED: 9/30/2025
SCALE: NTS
SHEET TITLE
**TYPICAL SECTIONS
C5.0**

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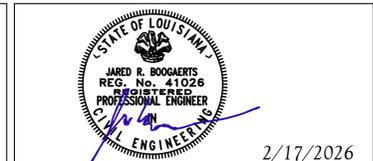


- LEGEND:**
- REMOVAL OF CONCRETE PAVEMENT & BASE
 - REMOVAL OF ASPHALT PAVEMENT & BASE
 - REMOVAL OF DRIVEWAY & BASE
 - REMOVAL OF STORM DRAIN PIPE OR STRUCTURE
 - REMOVAL OF WATER MAIN
 - REMOVAL OF EXISTING TREE
 - PARCEL BOUNDARY
 - EXISTING ASPHALT ROAD EDGE
 - EXISTING OVERHEAD POWER LINES
 - EXISTING FIRE HYDRANT
 - EXISTING POWER POLE
 - EXISTING WATER METER
 - EXISTING STORM DRAIN
 - EXISTING FENCE LINE
 - EXISTING SIGN
 - EXISTING WATER LINE
 - EXISTING SANITARY SEWER LINE
 - EXISTING SHRUB/HEDGE
 - EXISTING MAILBOX
 - EXISTING TREE

- KEY NOTES:**
- 1 REMOVE & RELOCATE EXISTING MAILBOX. SEE PAVE PLAN FOR PROPOSED PLACEMENT.
 - a) BRICK MAILBOX
 - b) METAL MAILBOX
 - c) PLASTIC MAILBOX
 - 2 SAW CUT CLEAN EDGE AS SHOWN.
 - 3 REMOVE EXISTING STORM DRAIN PIPE OR STRUCTURE WHERE SHOWN.
 - 4 REMOVE EXISTING TREE.
- CAUTION!!!**
EXISTING OVERHEAD ELECTRIC LINES AND EXISTING UNDERGROUND UTILITIES IN AREA TO REMAIN. PROTECT DURING CONSTRUCTION.

NO.	DATE	DESCRIPTION

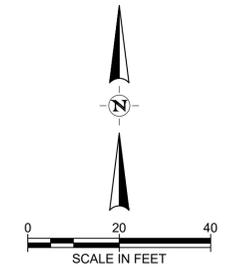
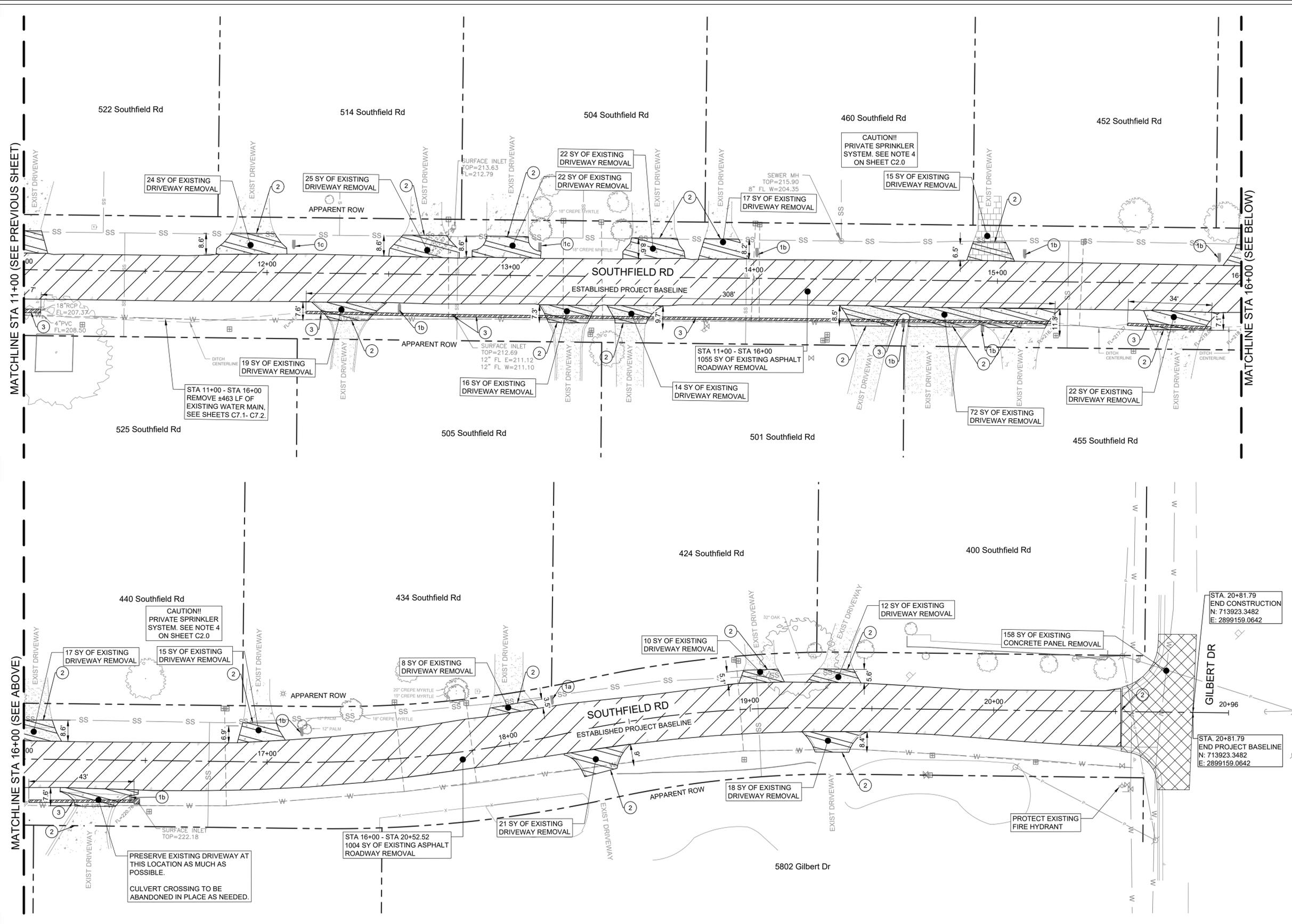
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DRAWN BY: JG
CHECKED BY: JB



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/26
SCALE: 1" = 20'
SHEET TITLE: DEMOLITION PLAN (1 OF 2)
C6.0

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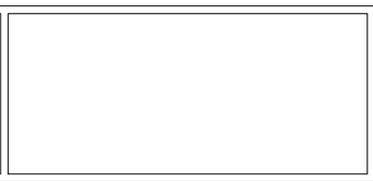


- LEGEND:**
- REMOVAL OF CONCRETE PAVEMENT & BASE
 - REMOVAL OF ASPHALT PAVEMENT & BASE
 - REMOVAL OF DRIVEWAY & BASE
 - REMOVAL OF STORM DRAIN PIPE OR STRUCTURE
 - REMOVAL OF WATER MAIN
 - REMOVAL OF EXISTING TREE
 - PARCEL BOUNDARY
 - EXISTING ASPHALT ROAD EDGE
 - EXISTING OVERHEAD POWER LINES
 - EXISTING FIRE HYDRANT
 - EXISTING POWER POLE
 - EXISTING WATER METER
 - EXISTING STORM DRAIN
 - EXISTING FENCE LINE
 - EXISTING SIGN
 - EXISTING WATER LINE
 - EXISTING SANITARY SEWER LINE
 - EXISTING SHRUB/HEDGE
 - EXISTING MAILBOX
 - EXISTING TREE

- KEY NOTES:**
- 1 REMOVE & RELOCATE EXISTING MAILBOX. SEE PAVE PLAN FOR PROPOSED PLACEMENT.
 - a BRICK MAILBOX
 - b METAL MAILBOX
 - c PLASTIC MAILBOX
 - 2 SAW CUT CLEAN EDGE AS SHOWN.
 - 3 REMOVE EXISTING STORM DRAIN PIPE OR STRUCTURE WHERE SHOWN.
 - 4 REMOVE EXISTING TREE.
- CAUTION!!!**
EXISTING OVERHEAD ELECTRIC LINES AND EXISTING UNDERGROUND UTILITIES IN AREA TO REMAIN. PROTECT DURING CONSTRUCTION.

REVISION		
NO.	DATE	DESCRIPTION

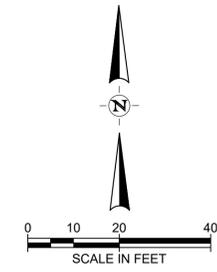
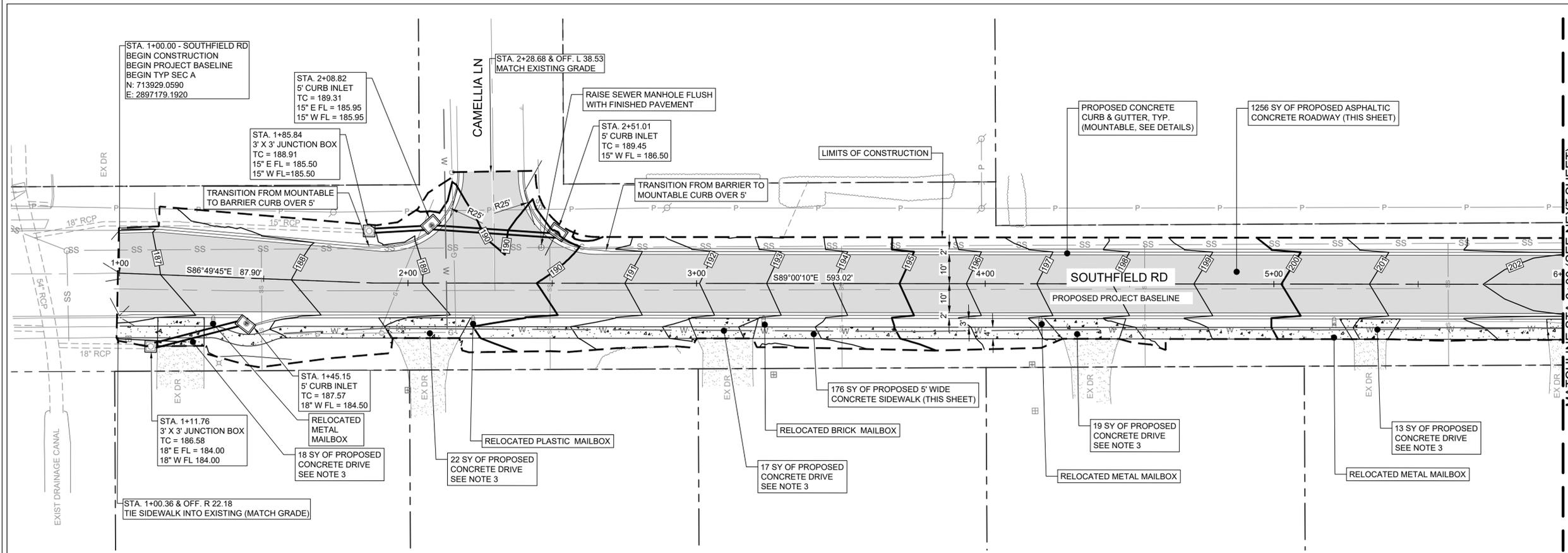
JG
DESIGNED BY:
JG
DRAWN BY:
JB
CHECKED BY:



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/26
SCALE: 1" = 20'
SHEET TITLE
DEMOLITION PLAN
(2 OF 2)
C6.1

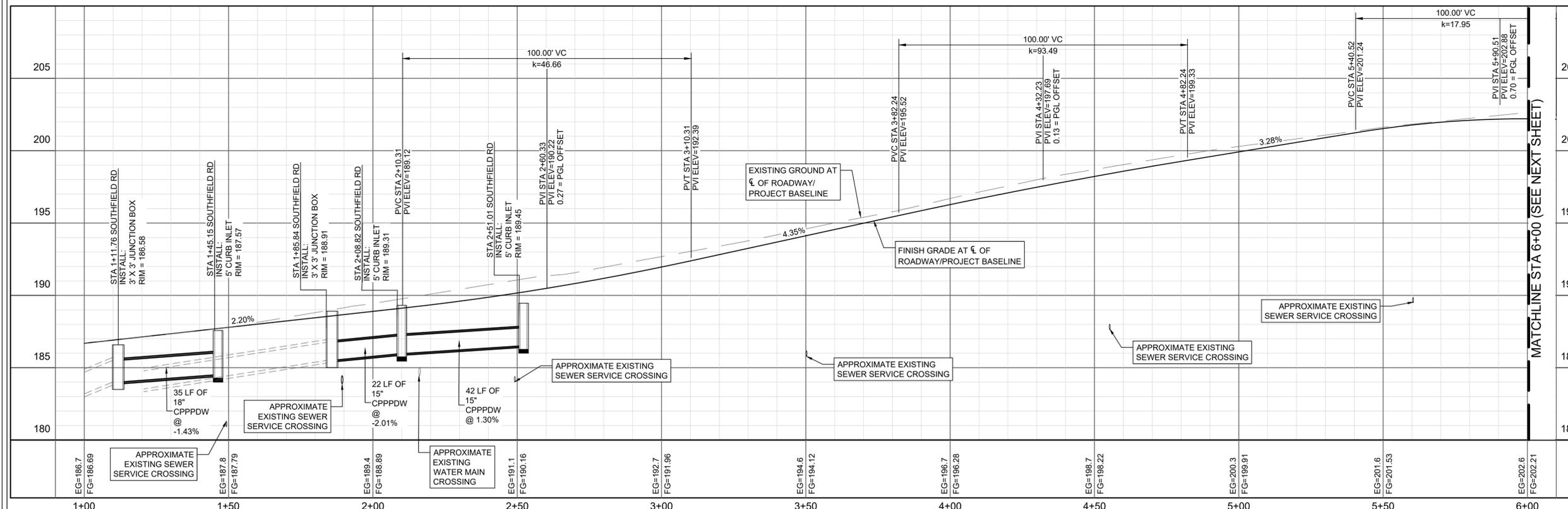
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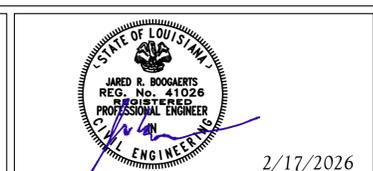
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	PROPOSED CONCRETE PAVEMENT
	PROPOSED CONCRETE CURB & GUTTER
	PROPOSED STORM DRAIN
	PARCEL BOUNDARY
	EXISTING OVERHEAD POWER LINES
	EXISTING FIRE HYDRANT
	EXISTING POWER POLE
	EXISTING WATER METER
	EXISTING STORM DRAIN
	EXISTING FENCE LINE
	EXISTING SIGN
	EXISTING WATER LINE
	EXISTING WASTE WATER LINE
	EXISTING SHRUB/HEDGE
	RELOCATED MAILBOX
	PROPOSED CONTOUR
	LIMITS OF CONSTRUCTION

- NOTES:**
- EXISTING DITCH ON THE SOUTH SIDE OF SOUTHFIELD ROAD MAY HAVE EXISTING YARD DRAINS THAT ARE NOT IDENTIFIED ON THE PLANS. CONTRACTOR TO COORDINATE WITH PROPERTY OWNERS, LOCATE, AND RECONNECT ALL EXISTING YARD DRAINS TO THE PROPOSED STORM DRAIN SYSTEM W/ INSERT-A-TEE CONNECTIONS.
 - ALL NEW WATER LINE TO BE A MINIMUM OF 3' BELOW FINISHED GRADE AND TO HAVE 18" MINIMUM HORIZONTAL AND VERTICAL SEPARATION FROM STORM DRAIN.
 - DRIVEWAY APRONS ARE TO BE CONCRETE PER SHEET C10.0 UNLESS OTHERWISE INDICATED. CONTRACTOR MAY PROVIDE ALTERNATIVE MATERIAL AT REQUEST OF THE PROPERTY OWNER AT NO ADDITIONAL COST TO THE CITY.
 - ALL H/C RAMP ARE TO BE TYPE 7. SEE DETAIL ON SHEET C10.0.
- CAUTION!!!**
EXISTING OVERHEAD ELECTRIC LINES AND EXISTING UNDERGROUND UTILITIES IN AREA TO REMAIN. PROTECT DURING CONSTRUCTION.

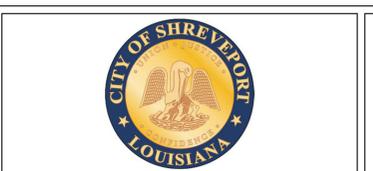


REVISION		
NO.	DATE	DESCRIPTION

JR
DESIGNED BY:
JR
DRAWN BY:
JB
CHECKED BY:



2/17/2026



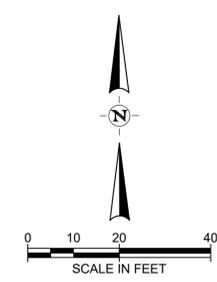
SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 9/30/25
SCALE: 1" = 20'
SHEET TITLE
PAVEMENT PLAN
(1 OF 4)
C7.0

FILE NAME: A:\47000\47786\038\DWG\CADD\Sheet\C-PLAN-C400-PAVE-47786-038.dwg DATE: February 17, 2026, TIME: 5:02 PM, USER: ah5754

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MATCHLINE STA 11+00 (SEE NEXT SHEET)

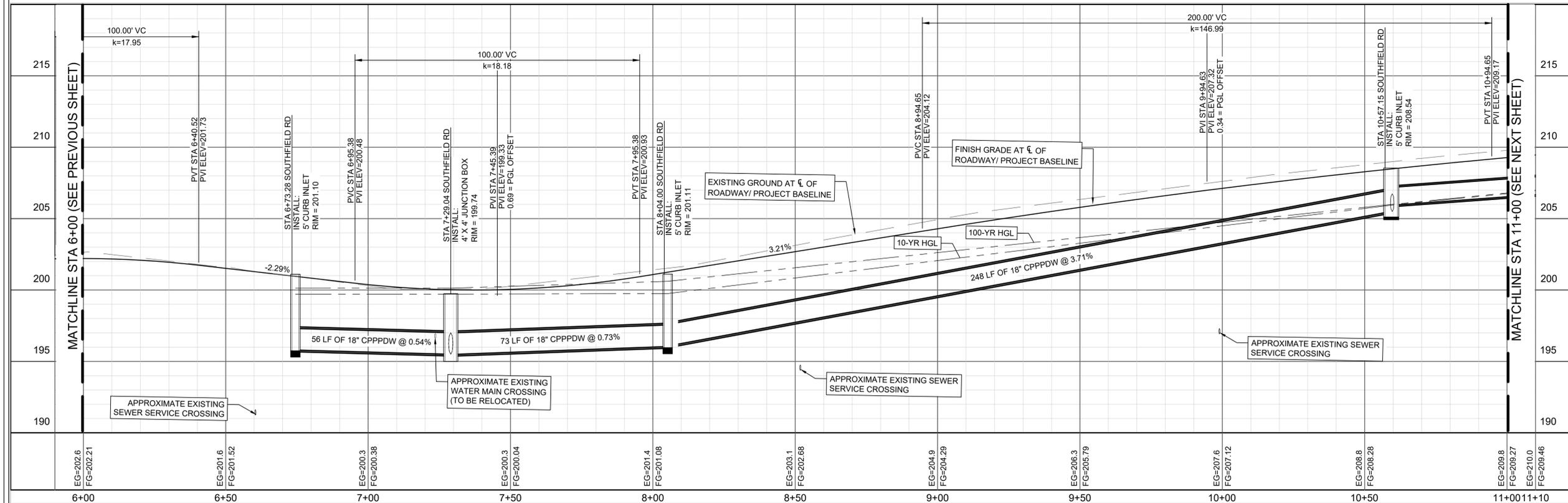
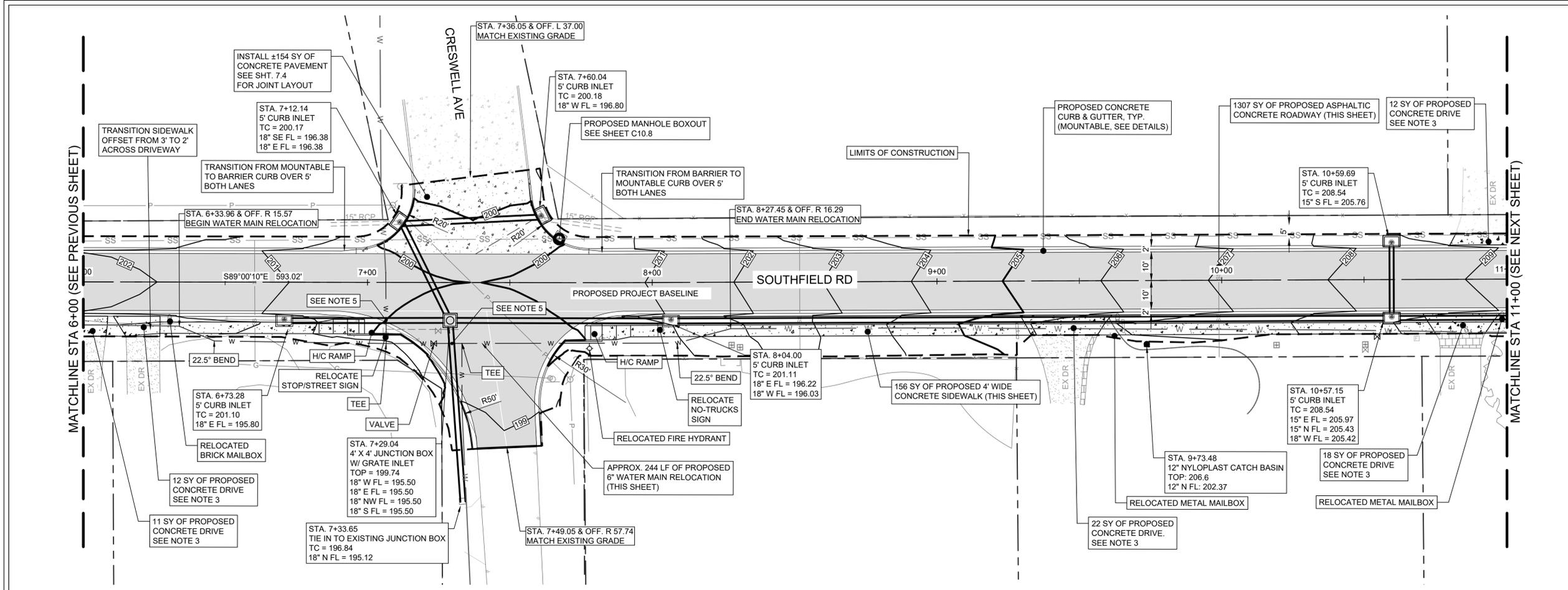


LEGEND:

- PROPOSED ASPHALT PAVEMENT
- PROPOSED CONCRETE PAVEMENT
- PROPOSED CONCRETE CURB & GUTTER
- PROPOSED STORM DRAIN
- PARCEL BOUNDARY
- EXISTING OVERHEAD POWER LINES
- EXISTING FIRE HYDRANT
- EXISTING POWER POLE
- EXISTING WATER METER
- EXISTING STORM DRAIN
- EXISTING FENCE LINE
- EXISTING SIGN
- EXISTING WATER LINE
- EXISTING WASTE WATER LINE
- EXISTING SHRUB/HEDGE
- RELOCATED MAILBOX
- PROPOSED CONTOUR

- NOTES:**
- EXISTING DITCH ON THE SOUTH SIDE OF SOUTHFIELD ROAD MAY HAVE EXISTING YARD DRAINS THAT ARE NOT IDENTIFIED ON THE PLANS. CONTRACTOR TO COORDINATE WITH PROPERTY OWNERS, LOCATE, AND RECONNECT ALL EXISTING YARD DRAINS TO THE PROPOSED STORM DRAIN SYSTEM W/ INSERT-A-TEE CONNECTIONS.
 - ALL NEW WATER LINE TO BE A MINIMUM OF 3' BELOW FINISHED GRADE AND TO HAVE 18" MINIMUM HORIZONTAL AND VERTICAL SEPARATION FROM STORM DRAIN.
 - DRIVEWAY APRONS ARE TO BE CONCRETE PER SHEET C10.0 UNLESS OTHERWISE INDICATED. CONTRACTOR MAY PROVIDE ALTERNATIVE MATERIAL AT REQUEST OF THE PROPERTY OWNER AT NO ADDITIONAL COST TO THE CITY.
 - ALL H/C RAMP ARE TO BE TYPE 7. SEE DETAIL ON SHEET C10.0.
 - WHERE NEW WATER MAIN CROSSES PROPOSED STORM DRAIN MAINTAIN AT LEAST 18" VERTICAL SEPARATION

CAUTION!!!
EXISTING OVERHEAD ELECTRIC LINES AND EXISTING UNDERGROUND UTILITIES IN AREA TO REMAIN. PROTECT DURING CONSTRUCTION.



NO.	DATE	DESCRIPTION

DESIGNED BY: JR
DRAWN BY: JR
CHECKED BY: JB

401 MARKET ST.
SUITE 650
SHREVEPORT, LA 71101
TEL. (318) 716-6136

JARED R. BOOGHEARTS
REG. No. 41026
REGISTERED PROFESSIONAL ENGINEER
CIVIL ENGINEERING

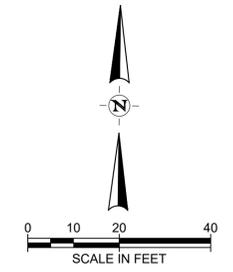
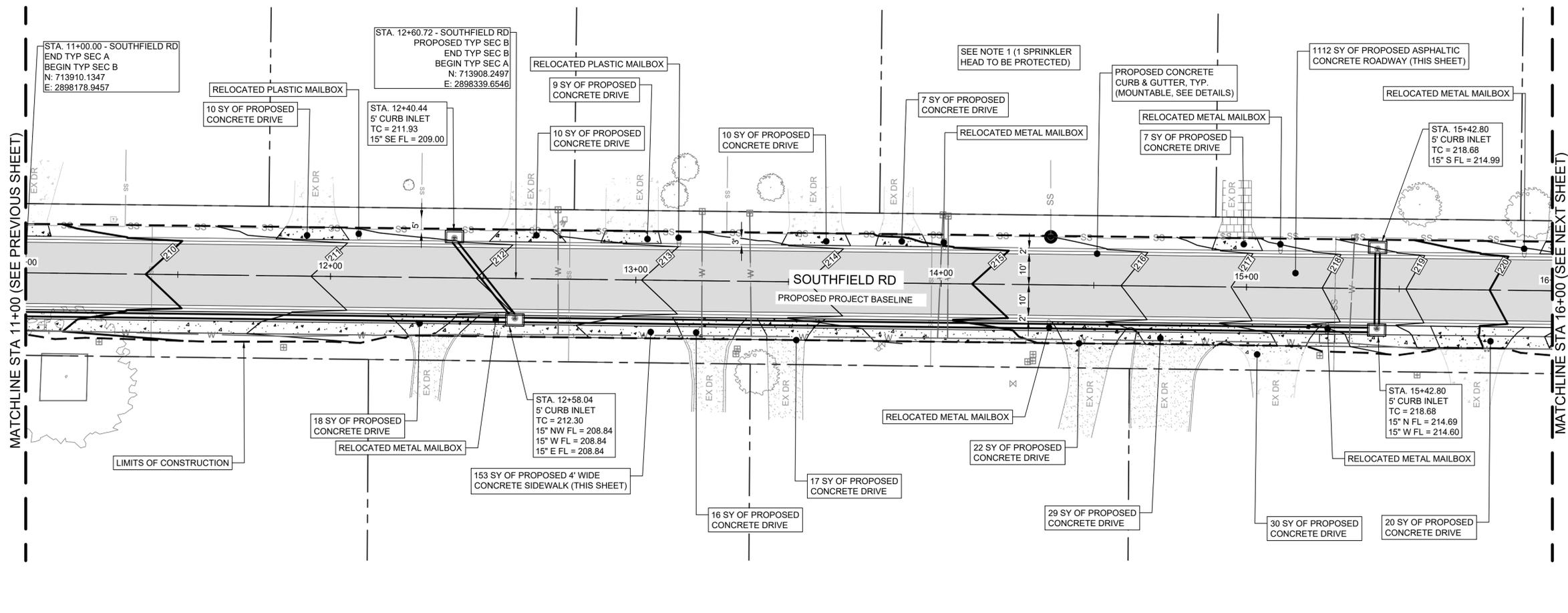
2/17/2026



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.:	47786.038
ISSUED:	9/30/25
SCALE:	1" = 20'
SHEET TITLE	PAVEMENT PLAN (2 OF 4)
	C7.1

FILE NAME: A:\47000\47786\038\1\W\CADD\Sheets\C-PLAN-C400-PAVE-47786-038.dwg DATE: February 17, 2026, TIME: 5:02 PM, USER: ah5764

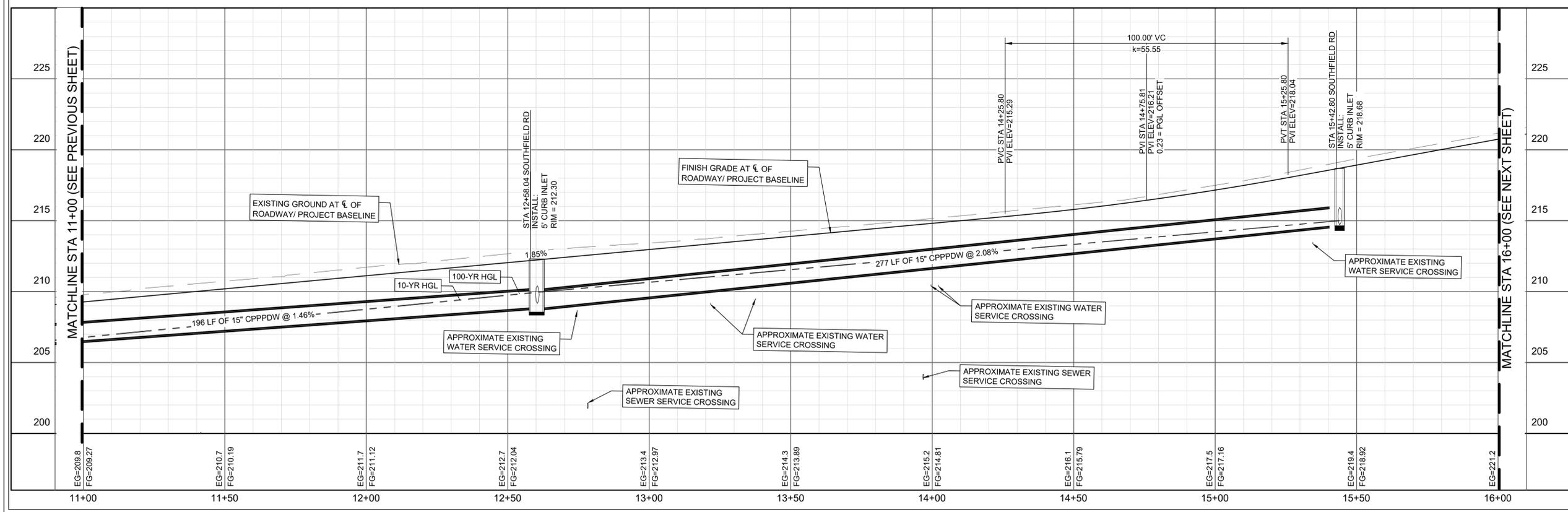


LEGEND:

	PROPOSED ASPHALT PAVEMENT
	PROPOSED CONCRETE PAVEMENT
	PROPOSED CONCRETE CURB & GUTTER
	PROPOSED STORM DRAIN
	PARCEL BOUNDARY
	EXISTING OVERHEAD POWER LINES
	EXISTING FIRE HYDRANT
	EXISTING POWER POLE
	EXISTING WATER METER
	EXISTING STORM DRAIN
	EXISTING FENCE LINE
	EXISTING SIGN
	EXISTING WATER LINE
	EXISTING WASTE WATER LINE
	EXISTING SHRUB/HEDGE
	RELOCATED MAILBOX
	PROPOSED CONTOUR

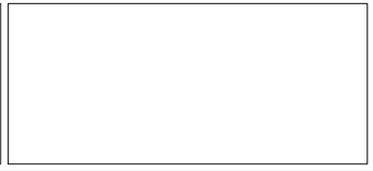
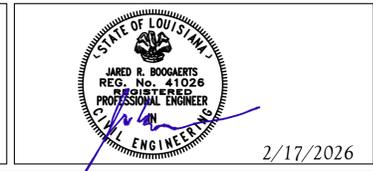
- NOTES:**
- EXISTING DITCH ON THE SOUTH SIDE OF SOUTHFIELD ROAD MAY HAVE EXISTING YARD DRAINS THAT ARE NOT IDENTIFIED ON THE PLANS. CONTRACTOR TO COORDINATE WITH PROPERTY OWNERS, LOCATE, AND RECONNECT ALL EXISTING YARD DRAINS TO THE PROPOSED STORM DRAIN SYSTEM W/ INSERT-A-TEE CONNECTIONS.
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 - ALL H/C RAMPS ARE TO BE TYPE 7. SEE DETAIL ON SHEET C10.0.

CAUTION!!!
EXISTING OVERHEAD ELECTRIC LINES AND EXISTING UNDERGROUND UTILITIES IN AREA TO REMAIN. PROTECT DURING CONSTRUCTION.



NO.	DATE	DESCRIPTION

JR
DESIGNED BY:
JR
DRAWN BY:
JB
CHECKED BY:



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.:	47786.038
ISSUED:	9/30/25
SCALE:	1" = 20'
SHEET TITLE	PAVEMENT PLAN
	(3 OF 4)
	C7.2

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MATCHLINE STA 16+00 (SEE PREVIOUS SHEET)

16+00

16+50

17+00

17+50

18+00

18+50

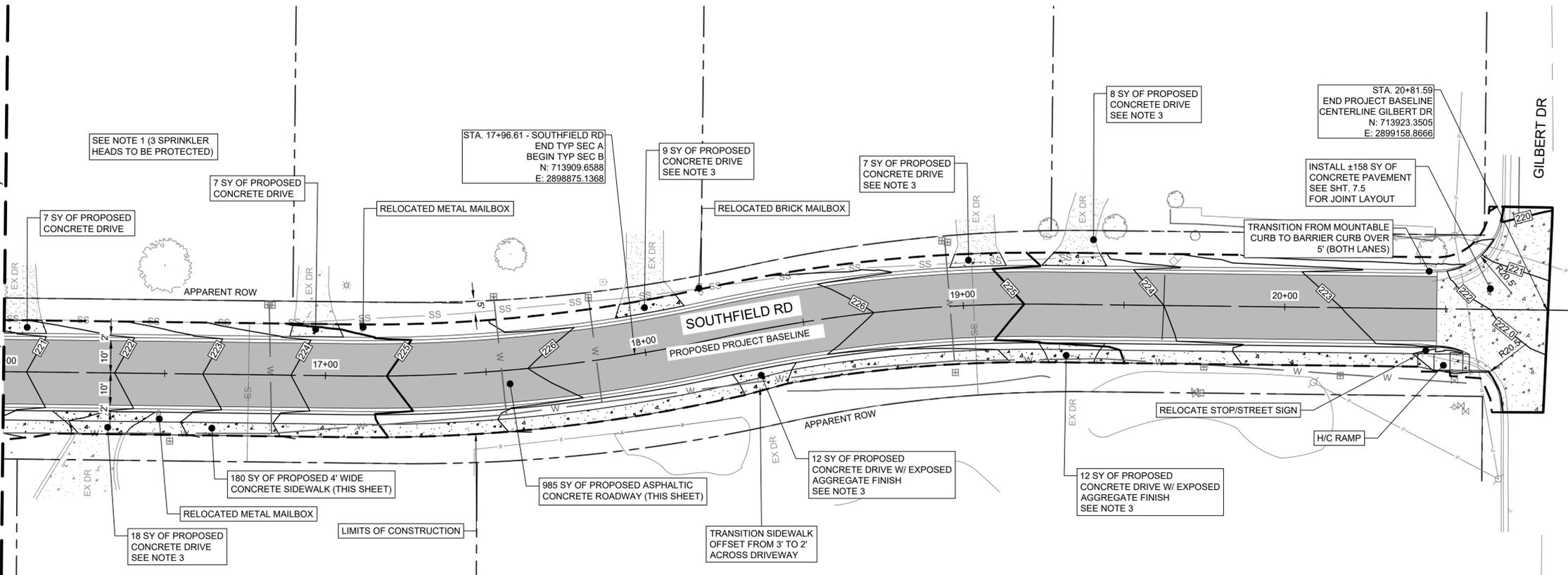
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19+50

20+00

20+50

21+00



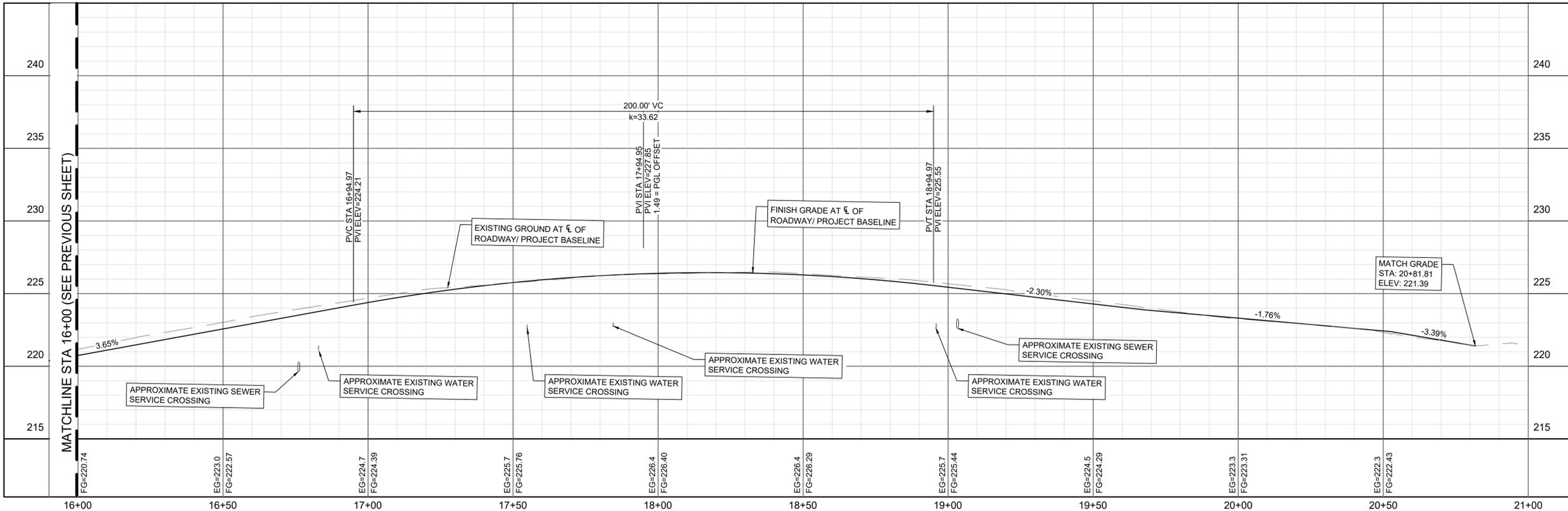
LEGEND:

- PROPOSED ASPHALT PAVEMENT
- PROPOSED CONCRETE PAVEMENT
- PROPOSED CONCRETE CURB & GUTTER
- PROPOSED STORM DRAIN
- PARCEL BOUNDARY
- EXISTING OVERHEAD POWER LINES
- EXISTING FIRE HYDRANT
- EXISTING POWER POLE
- EXISTING WATER METER
- EXISTING STORM DRAIN
- EXISTING FENCE LINE
- EXISTING SIGN
- EXISTING WATER LINE
- EXISTING WASTE WATER LINE
- EXISTING SHRUB/HEDGE
- RELOCATED MAILBOX
- PROPOSED CONTOUR

NOTES:

- EXISTING DITCH ON THE SOUTH SIDE OF SOUTHFIELD ROAD MAY HAVE EXISTING YARD DRAINS THAT ARE NOT IDENTIFIED ON THE PLANS. CONTRACTOR TO COORDINATE WITH PROPERTY OWNERS, LOCATE, AND RECONNECT ALL EXISTING YARD DRAINS TO THE PROPOSED STORM DRAIN SYSTEM W/ INSERT-A-TEE CONNECTIONS.
- ALL NEW WATER LINE TO BE A MINIMUM OF 3' BELOW FINISHED GRADE AND TO HAVE 18" MINIMUM HORIZONTAL AND VERTICAL SEPARATION FROM STORM DRAIN.
- DRIVEWAY APRONS ARE TO BE CONCRETE PER SHEET C10.0 UNLESS OTHERWISE INDICATED. CONTRACTOR MAY PROVIDE ALTERNATIVE MATERIAL AT REQUEST OF THE PROPERTY OWNER AT NO ADDITIONAL COST TO THE CITY.
- ALL H/C RAMP ARE TO BE TYPE 7. SEE DETAIL ON SHEET C10.0.

CAUTION!!!
EXISTING OVERHEAD ELECTRIC LINES AND EXISTING UNDERGROUND UTILITIES IN AREA TO REMAIN. PROTECT DURING CONSTRUCTION.



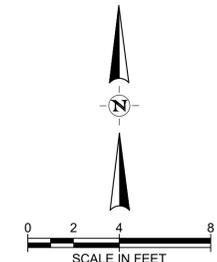
NO.	DATE	DESCRIPTION

JR
DESIGNED BY:
JR
DRAWN BY:
JB
CHECKED BY:



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 9/30/25
SCALE: 1" = 20'
SHEET TITLE
PAVEMENT PLAN
(4 OF 4)
C7.3



LEGEND:

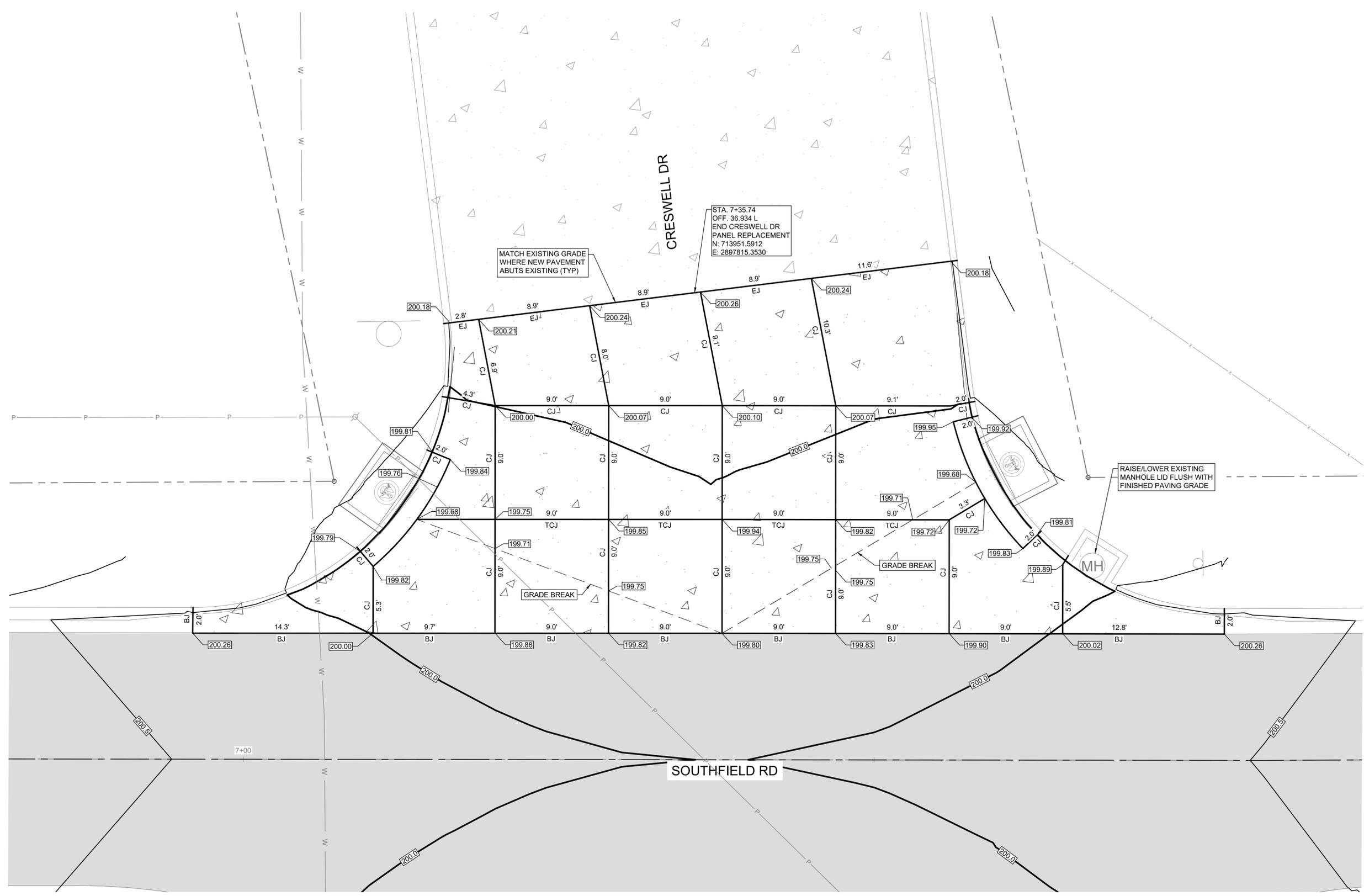
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	PROPOSED JOINTS
	PROPOSED CONCRETE CURB & GUTTER
	PROPOSED STORM DRAIN
	PARCEL BOUNDARY
	EXISTING OVERHEAD POWER LINES
	EXISTING FIRE HYDRANT
	EXISTING POWER POLE
	EXISTING WATER METER
	EXISTING STORM DRAIN
	EXISTING FENCE LINE
	EXISTING SIGN
	EXISTING WATER LINE
	EXISTING WASTE WATER LINE
	EXISTING SHRUB/HEDGE
	RELOCATED MAILBOX

- NOTES:**
- NOTE. A CLEAN STRAIGHT FULL DEPTH SAW CUT IS REQUIRED TO OBTAIN A SMOOTH AND NEAT EDGE BEFORE REMOVAL OF EXISTING CONCRETE.
 - NOTE. DIMENSIONS SHOWN ARE FOR NEW CONCRETE PANELS AND ARE FOR INFORMATIONAL PURPOSES ONLY. CONTRACTOR IS TO VERIFY DIMENSIONS OF EACH PANEL REPLACEMENT AND VERIFY JOINT ALIGNMENT WHERE PRACTICABLE.
 - NOTE. REMOVE AND REPLACE 743 SY OF PORTLAND CEMENT CONCRETE PANELS
 - CONTRACTOR TO PROTECT ANY EXISTING PANELS NOT CALLED TO BE REMOVED. ANY DAMAGES TO EXISTING PANELS TO REMAIN IN PLACE ARE TO BE REMOVED AND REPLACED AT NO ADDITIONAL COST TO THE OWNER.
 - ALL UTILITIES SHOWN ARE BASED OFF OF GIS DATA, AND UTILITY COMPANY DRAWINGS. CONTRACTOR SHALL FIELD VERIFY ALL UTILITIES DURING CONSTRUCTION.
 - MATCH EXISTING GRADES WHERE ALL NEW PANELS ADJOIN EXISTING PANELS.

JOINT ABBREVIATIONS

LJ	-	LONGITUDINAL JOINT
EJ	-	TRANSVERSE EXPANSION JOINT
TCJ	-	TRANSVERSE CONTRACTION JOINT
CJ	-	CONSTRUCTION JOINT
L CJ	-	LONGITUDINAL CONSTRUCTION JOINT
LBJ	-	LONGITUDINAL BUTT JOINT
BJ	-	TRANSVERSE BUTT JOINT

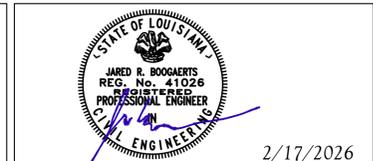
CAUTION!!!
EXISTING OVERHEAD ELECTRIC LINES AND EXISTING UNDERGROUND UTILITIES IN AREA TO REMAIN. PROTECT DURING CONSTRUCTION.



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NO.	DATE	DESCRIPTION

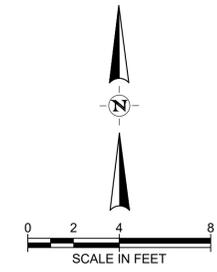
JR
DESIGNED BY:
JR
DRAWN BY:
JB
CHECKED BY:



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/2026
SCALE: 1" = 80'
SHEET TITLE: CRESWELL JOINT LAYOUT
C7.4

FILE NAME: A:\47000\47786\038\DWG\CADD\Sheet\C-PLAN-C700-INTS-47786-038.dwg DATE: February 17, 2026, TIME: 5:03 PM, USER: ah5754



LEGEND:

- PROPOSED ASPHALT PAVEMENT
- PROPOSED CONCRETE PAVEMENT
- PROPOSED JOINTS
- PROPOSED CONCRETE CURB & GUTTER
- PROPOSED STORM DRAIN
- PARCEL BOUNDARY
- EXISTING OVERHEAD POWER LINES
- EXISTING FIRE HYDRANT
- EXISTING POWER POLE
- EXISTING WATER METER
- EXISTING STORM DRAIN
- EXISTING FENCE LINE
- EXISTING SIGN
- EXISTING WATER LINE
- EXISTING WASTE WATER LINE
- EXISTING SHRUB/HEDGE
- RELOCATED MAILBOX

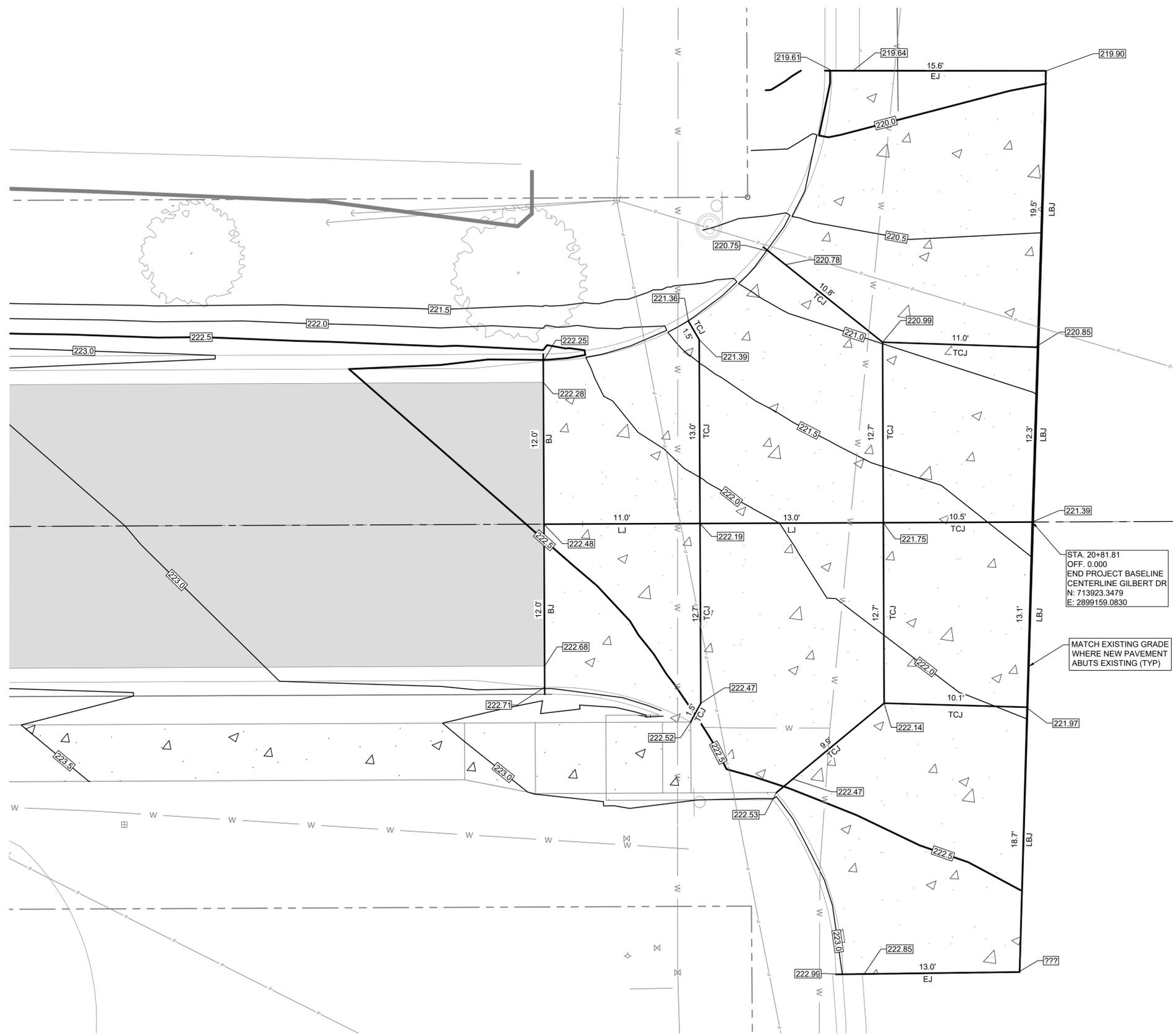
NOTES:

1. NOTE. A CLEAN STRAIGHT FULL DEPTH SAW CUT IS REQUIRED TO OBTAIN A SMOOTH AND NEAT EDGE BEFORE REMOVAL OF EXISTING CONCRETE.
2. NOTE. DIMENSIONS SHOWN ARE FOR NEW CONCRETE PANELS AND ARE FOR INFORMATIONAL PURPOSES ONLY. CONTRACTOR IS TO VERIFY DIMENSIONS OF EACH PANEL REPLACEMENT AND VERIFY JOINT ALIGNMENT WHERE PRACTICABLE.
3. NOTE. REMOVE AND REPLACE 743 SY OF PORTLAND CEMENT CONCRETE PANELS
4. CONTRACTOR TO PROTECT ANY EXISTING PANELS NOT CALLED TO BE REMOVED. ANY DAMAGES TO EXISTING PANELS TO REMAIN IN PLACE ARE TO BE REMOVED AND REPLACED AT NO ADDITIONAL COST TO THE OWNER.
5. ALL UTILITIES SHOWN ARE BASED OFF OF GIS DATA, AND UTILITY COMPANY DRAWINGS. CONTRACTOR SHALL FIELD VERIFY ALL UTILITIES DURING CONSTRUCTION.
6. MATCH EXISTING GRADES WHERE ALL NEW PANELS ADJOIN EXISTING PANELS.

JOINT ABBREVIATIONS

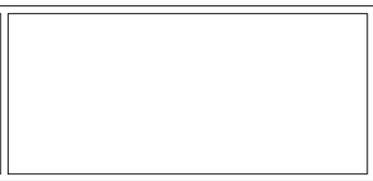
- LJ - LONGITUDINAL JOINT
- EJ - TRANSVERSE EXPANSION JOINT
- TCJ - TRANSVERSE CONTRACTION JOINT
- CJ - CONSTRUCTION JOINT
- LCJ - LONGITUDINAL CONSTRUCTION JOINT
- LBJ - LONGITUDINAL BUTT JOINT
- BJ - TRANSVERSE BUTT JOINT

CAUTION!!!
EXISTING OVERHEAD ELECTRIC LINES AND EXISTING UNDERGROUND UTILITIES IN AREA TO REMAIN. PROTECT DURING CONSTRUCTION.



NO.	DATE	DESCRIPTION

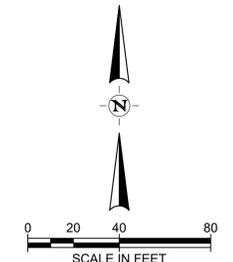
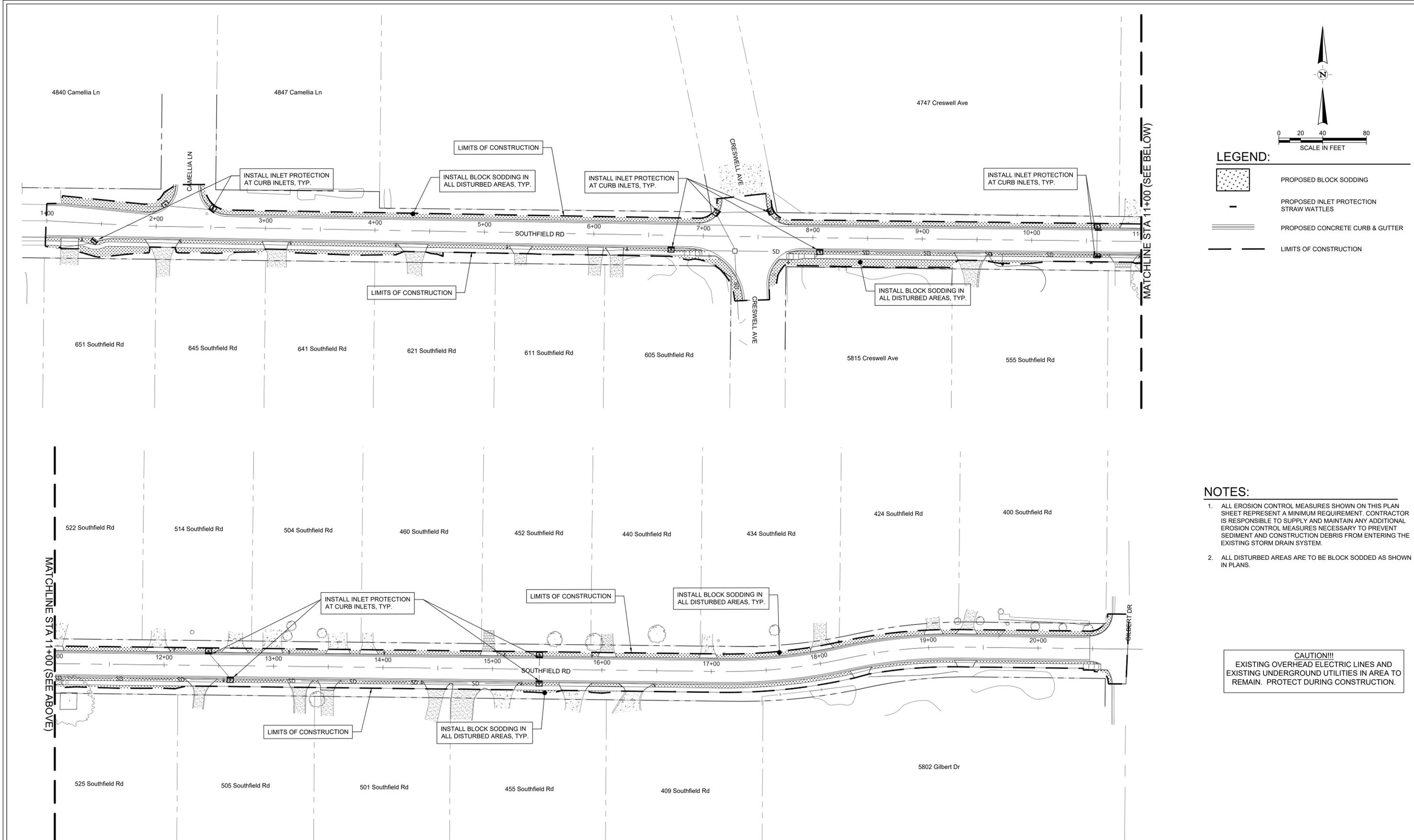
JR
DESIGNED BY:
JR
DRAWN BY:
JB
CHECKED BY:



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/6/2026
SCALE: 1" = 20'
SHEET TITLE:
GILBERT JOINT LAYOUT
C7.5

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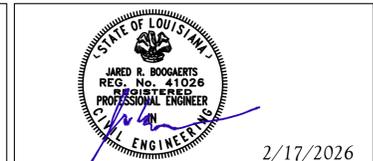
	PROPOSED BLOCK SODDING
	PROPOSED INLET PROTECTION STRAW WATTLES
	PROPOSED CONCRETE CURB & GUTTER
	LIMITS OF CONSTRUCTION

- NOTES:**
- ALL EROSION CONTROL MEASURES SHOWN ON THIS PLAN SHEET REPRESENT A MINIMUM REQUIREMENT. CONTRACTOR IS RESPONSIBLE TO SUPPLY AND MAINTAIN ANY ADDITIONAL EROSION CONTROL MEASURES NECESSARY TO PREVENT SEDIMENT AND CONSTRUCTION DEBRIS FROM ENTERING THE EXISTING STORM DRAIN SYSTEM.
 - ALL DISTURBED AREAS ARE TO BE BLOCK SODDED AS SHOWN IN PLANS.

CAUTION!!!
EXISTING OVERHEAD ELECTRIC LINES AND EXISTING UNDERGROUND UTILITIES IN AREA TO REMAIN. PROTECT DURING CONSTRUCTION.

REVISION		
NO.	DATE	DESCRIPTION

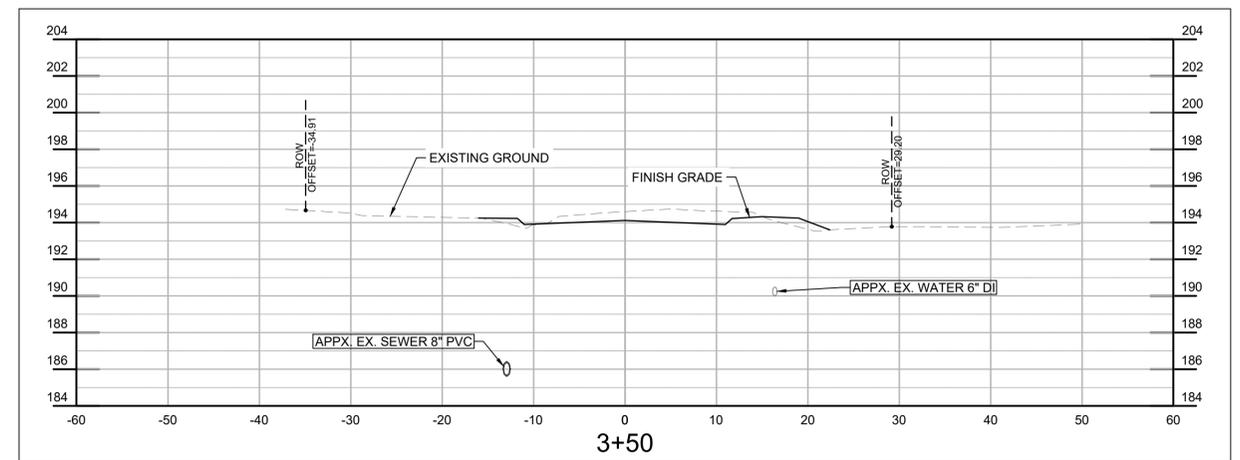
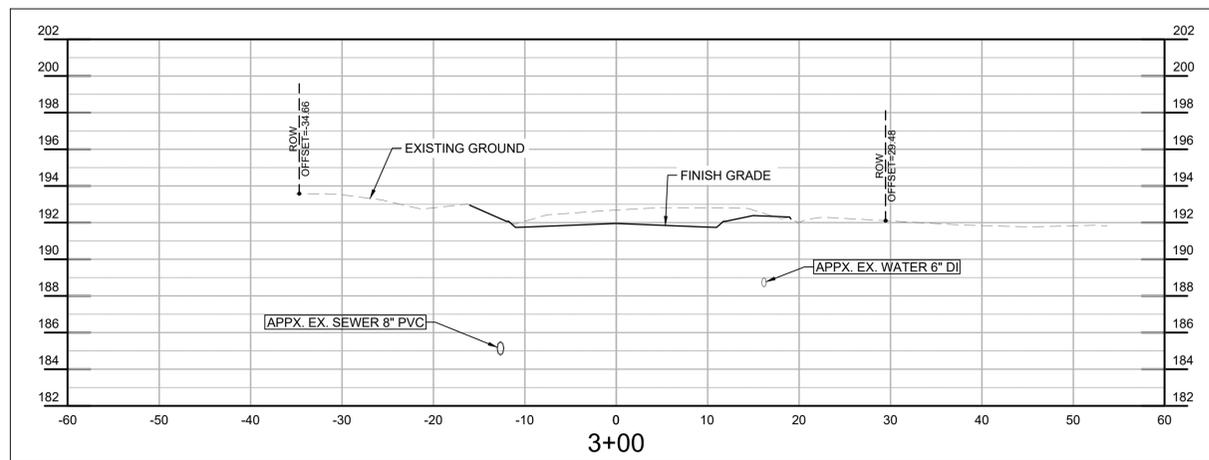
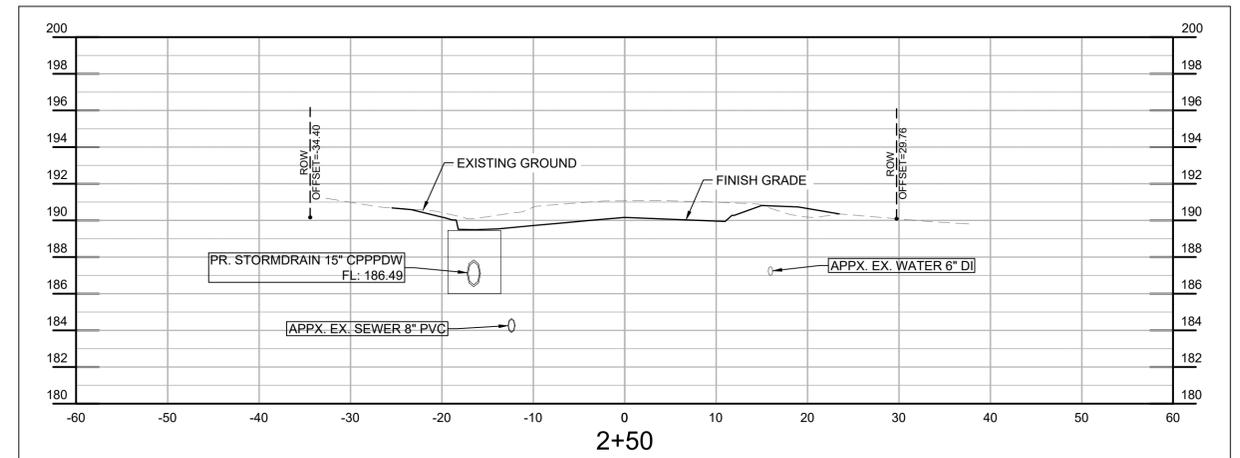
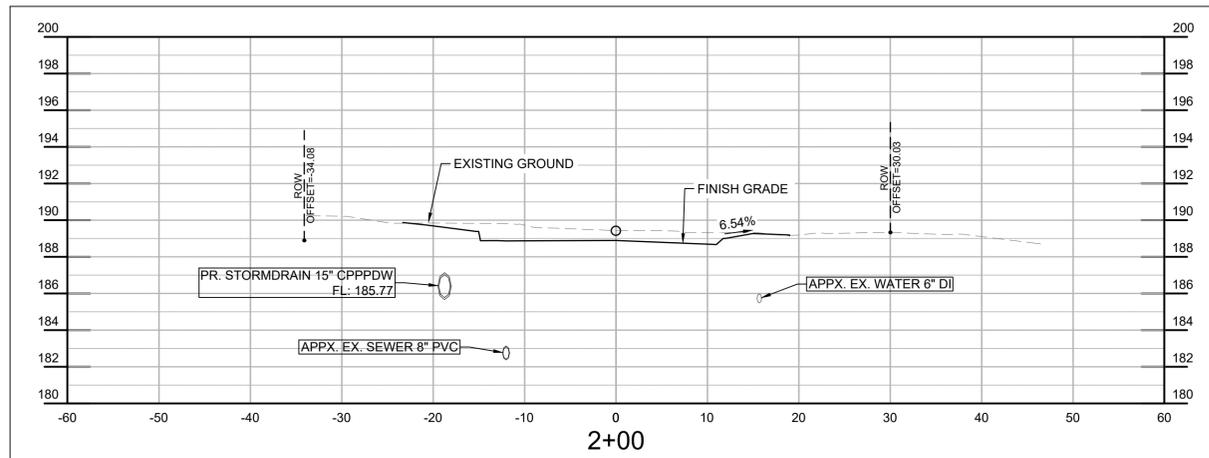
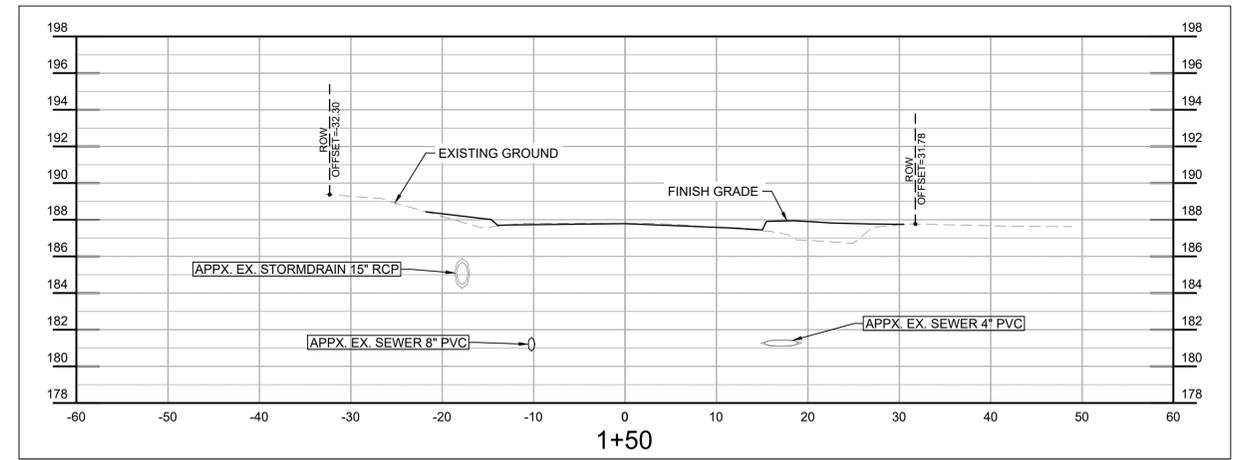
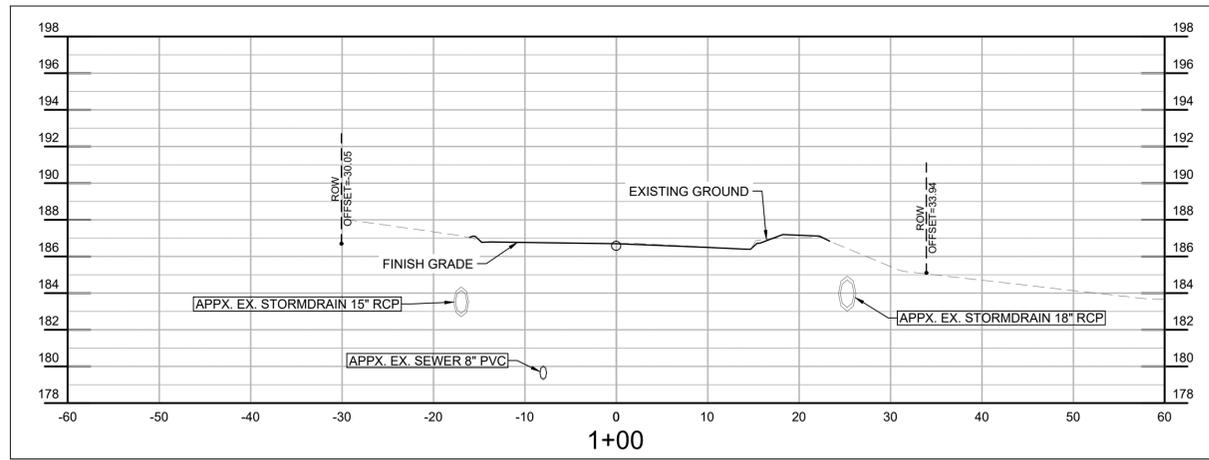
DESIGNED BY: JG
DRAWN BY: JB
CHECKED BY: JB



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDOPARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/26
SCALE: AS SHOWN
SHEET TITLE
EROSION CONTROL PLAN
C8.0

FILE NAME: A:\4700\47786\038\DWG\CADD\Sheet\C-PLAN-C500-XSEC-47786-038.dwg DATE: February 17, 2026, TIME: 5:04 PM, USER: ah5754



REVISION NO.	DATE	DESCRIPTION

DESIGNED BY: JKR
 DRAWN BY: JKR
 CHECKED BY: JB



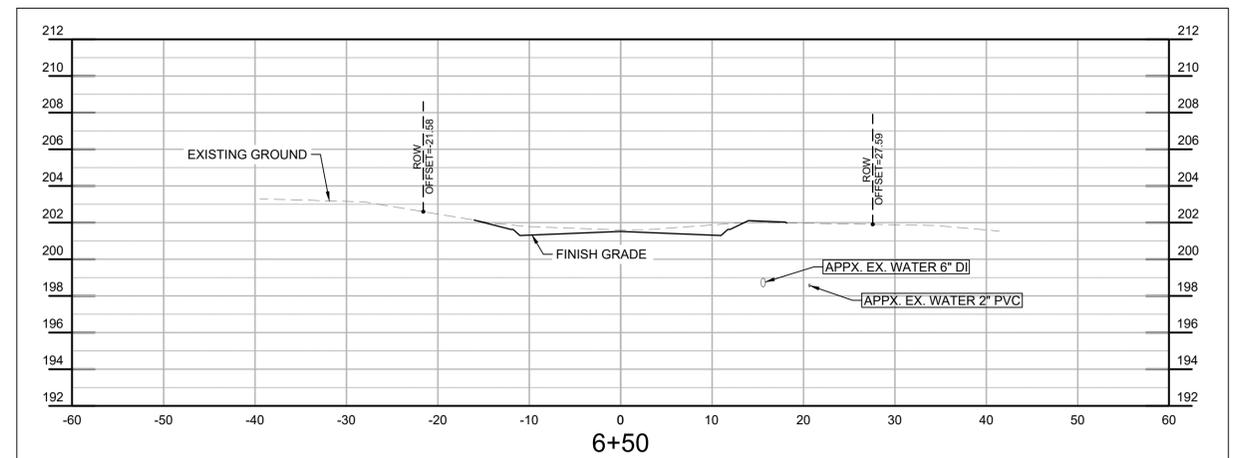
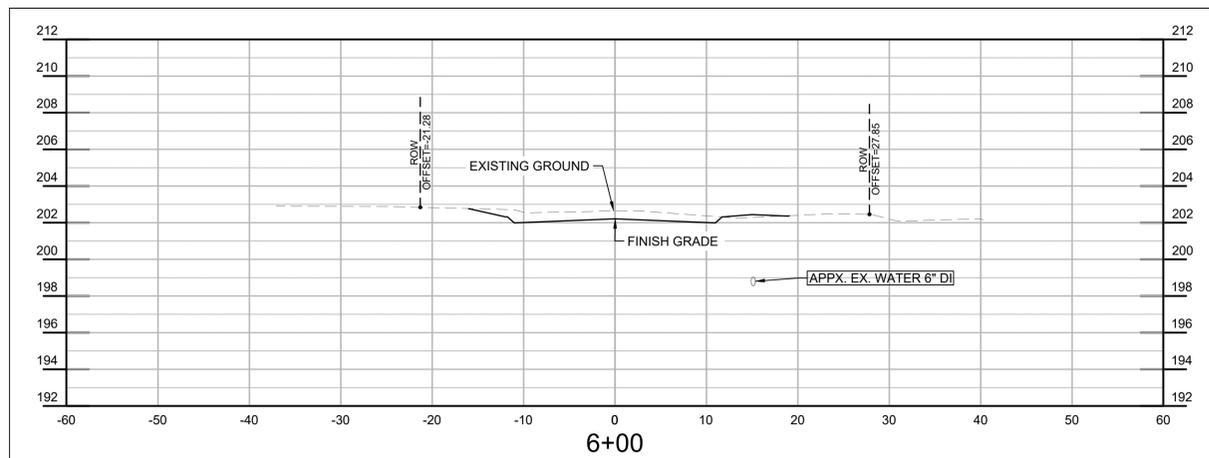
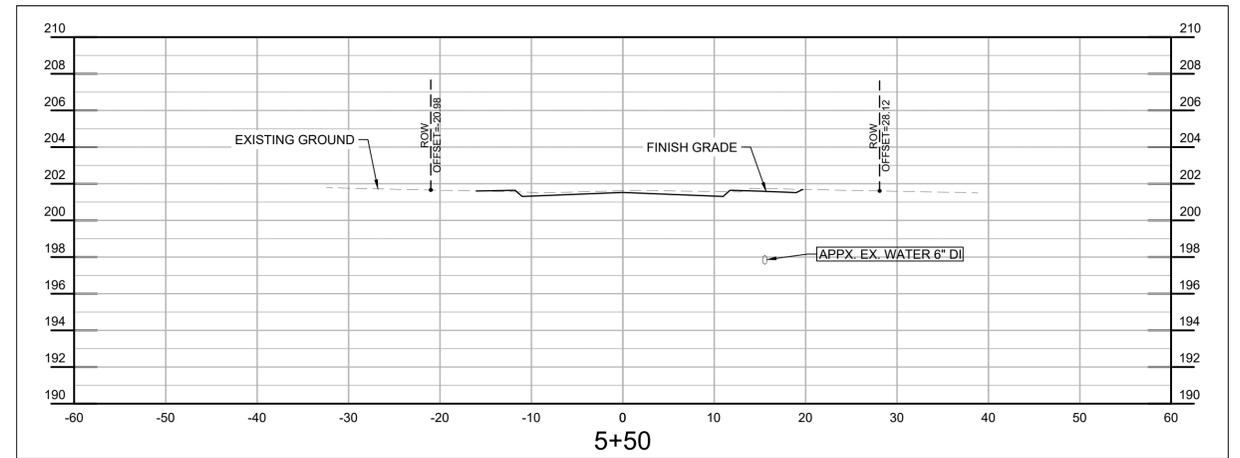
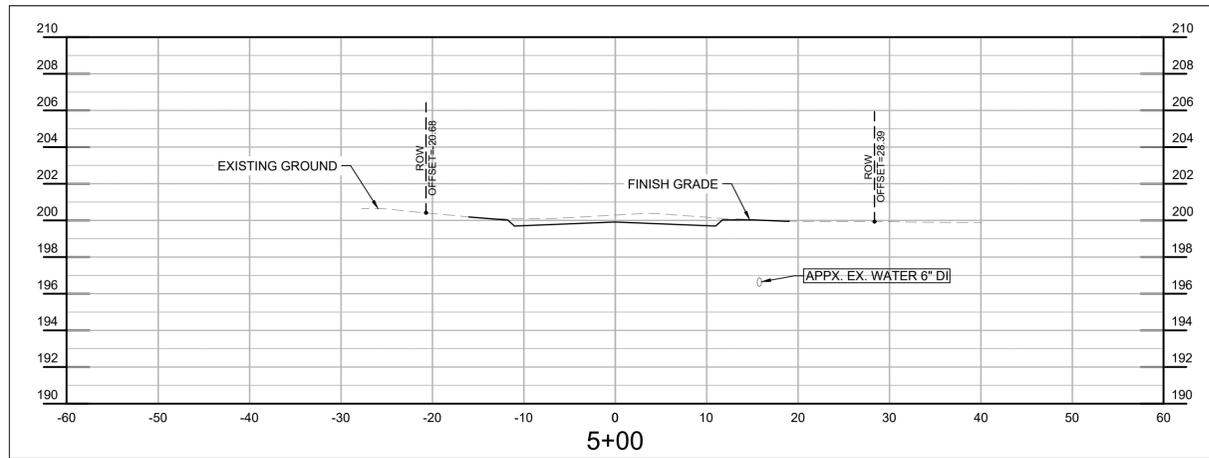
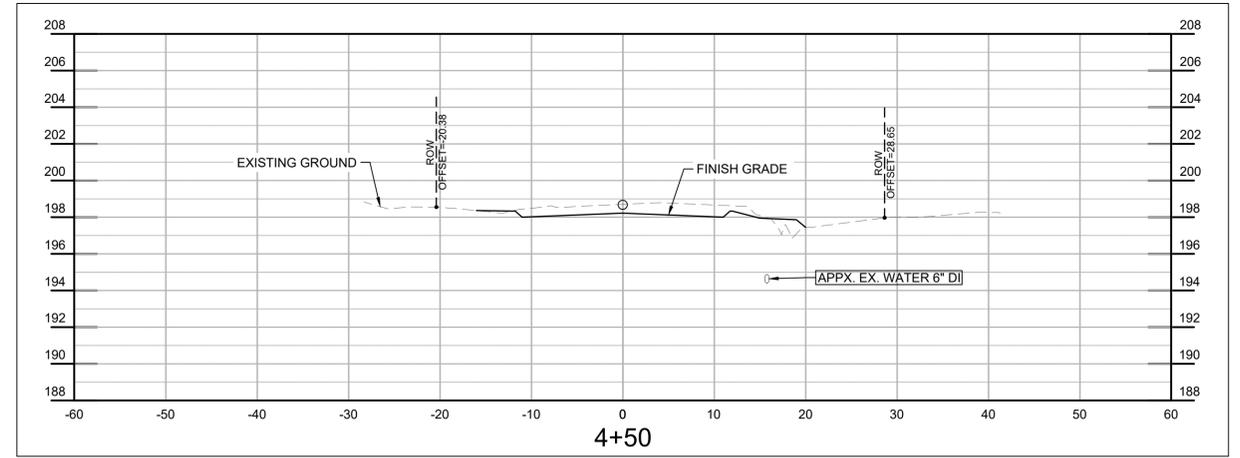
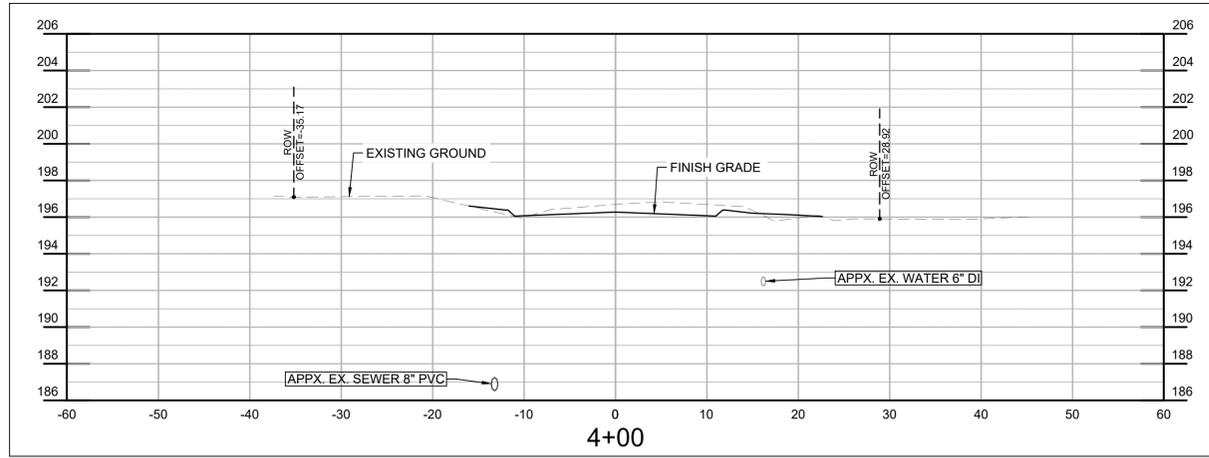
2/17/2026



SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

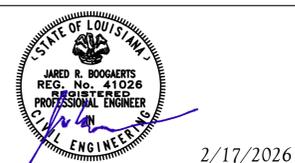
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 ISSUED: 2/17/26
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 C9.0

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NO.	DATE	DESCRIPTION

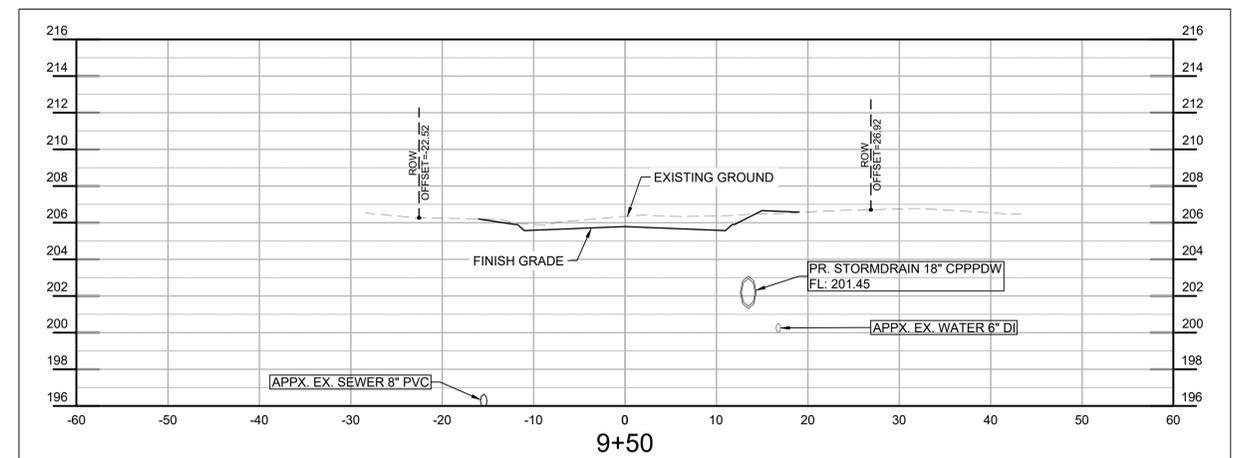
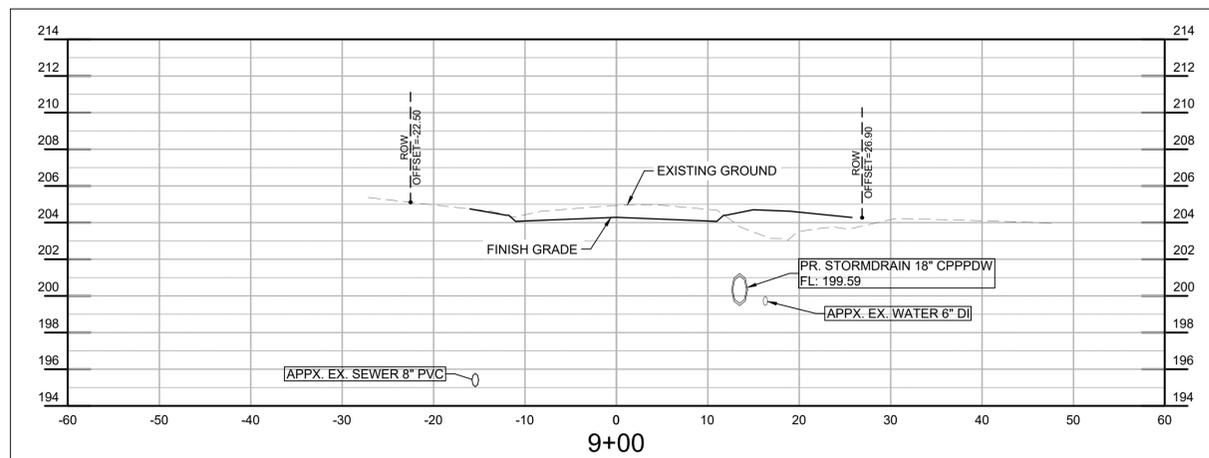
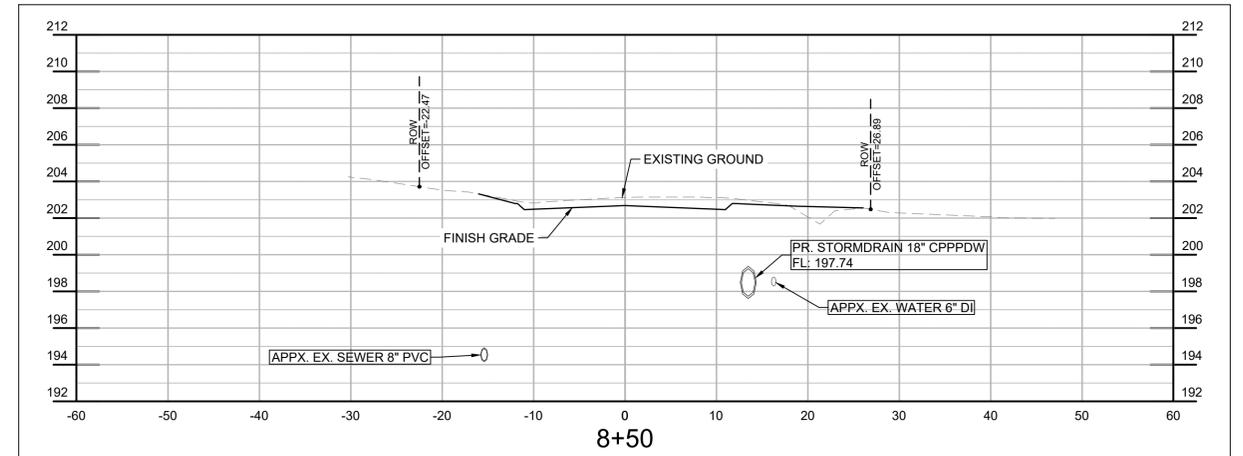
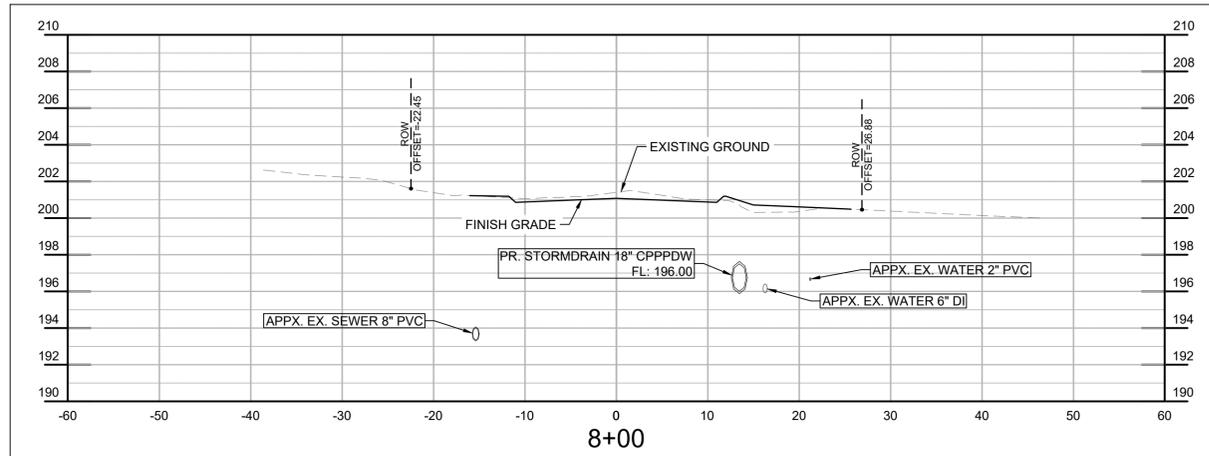
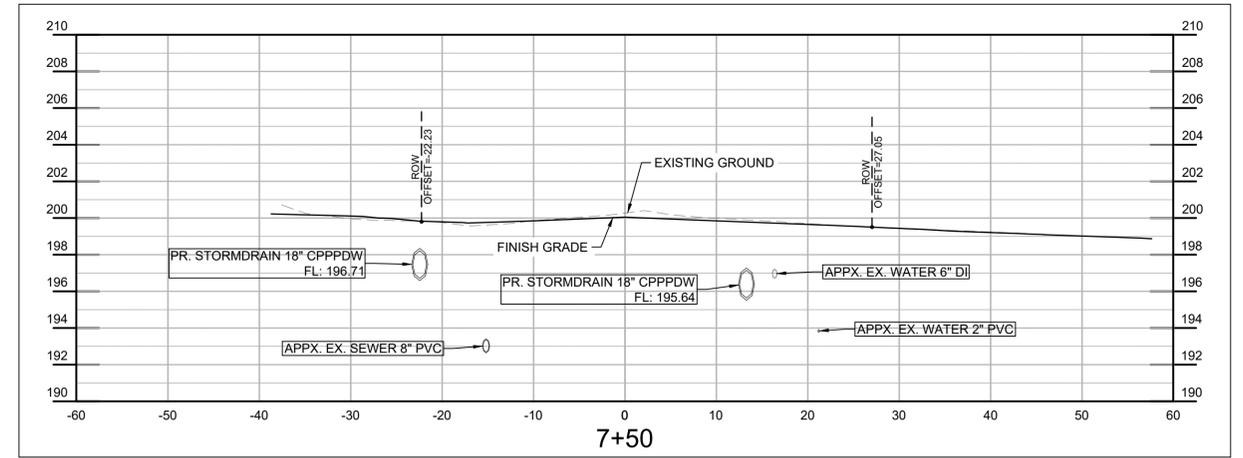
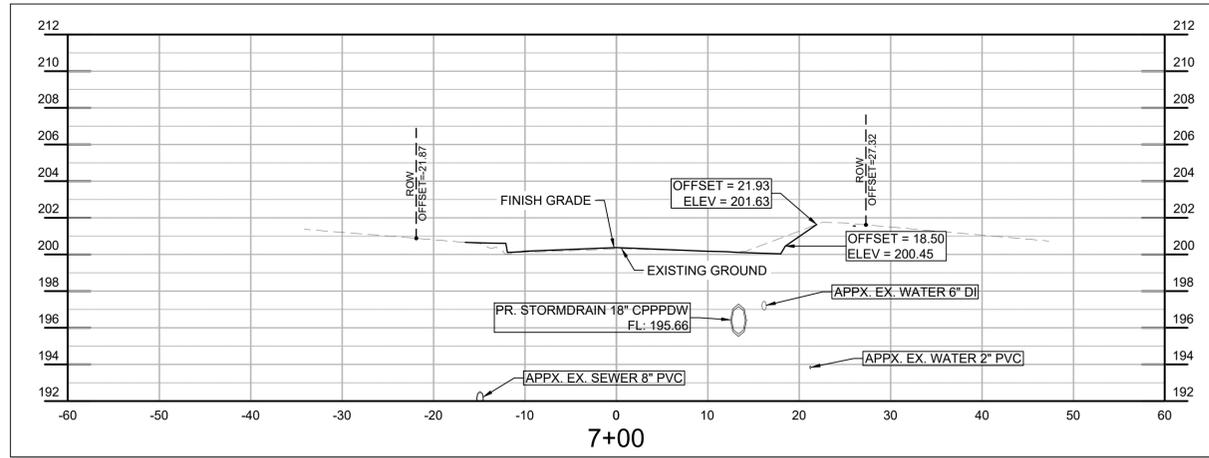
DESIGNED BY: JKR
 DRAWN BY: JKR
 CHECKED BY: JB



SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
 ISSUED: 2/17/26
 SCALE: 1" = 10'
 SHEET TITLE
 CROSS SECTIONS
 (2 OF 7)
 C9.1

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REVISION NO.	DATE	DESCRIPTION

JKR
DESIGNED BY:
JKR
DRAWN BY:
JB
CHECKED BY:



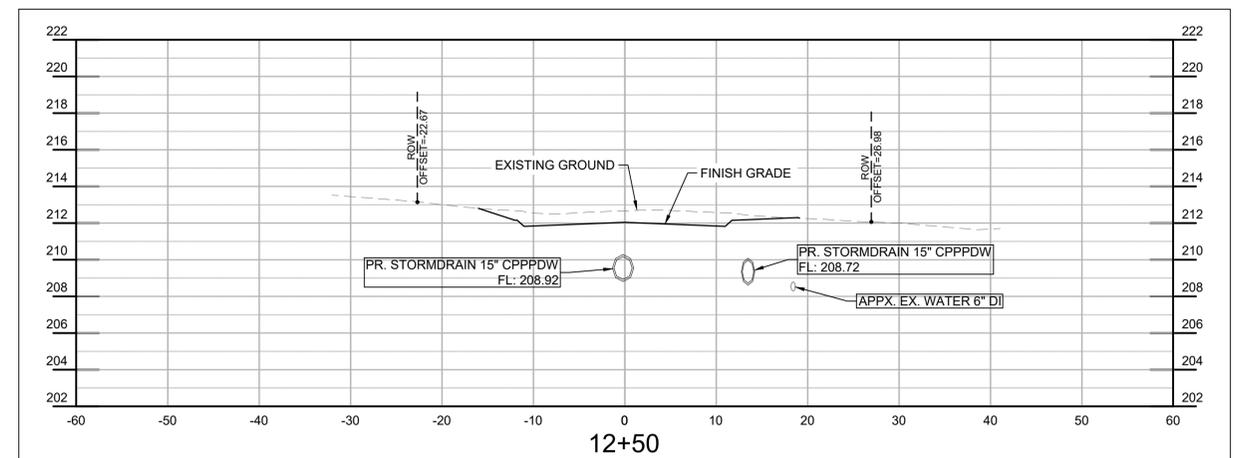
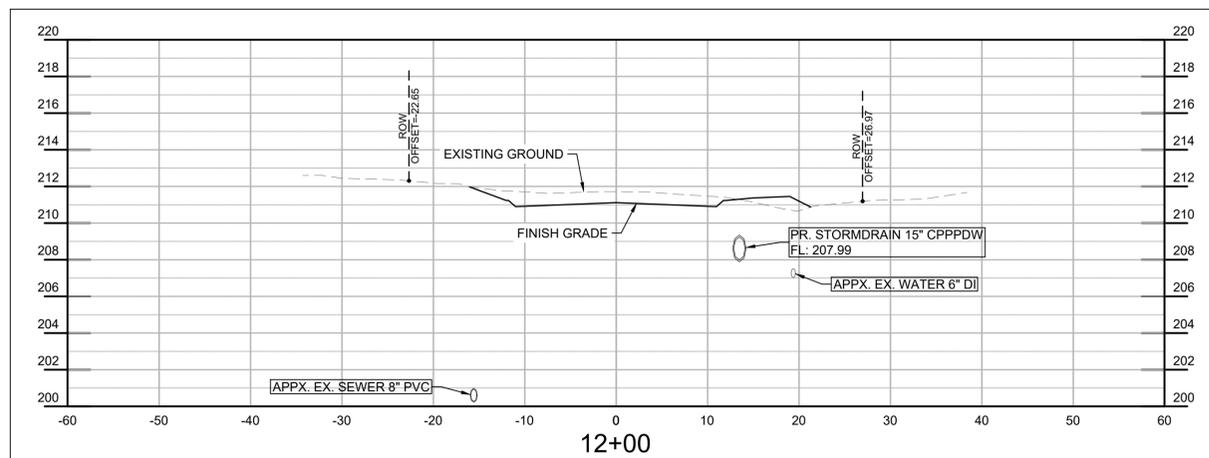
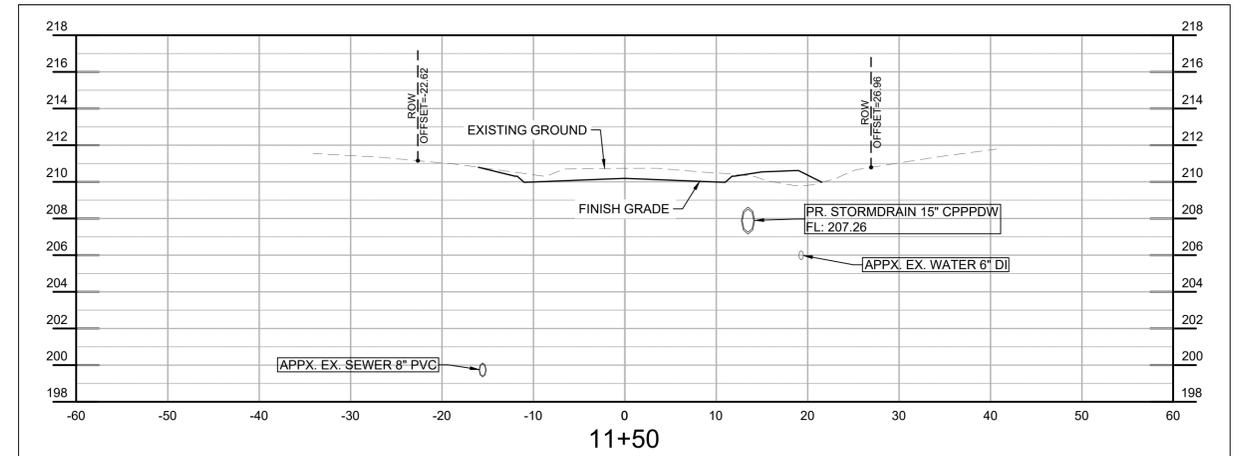
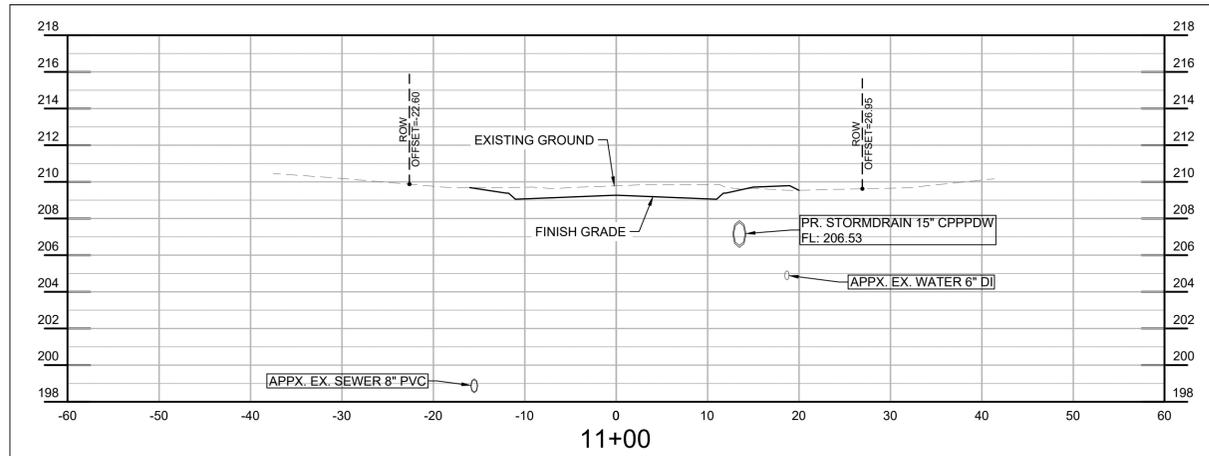
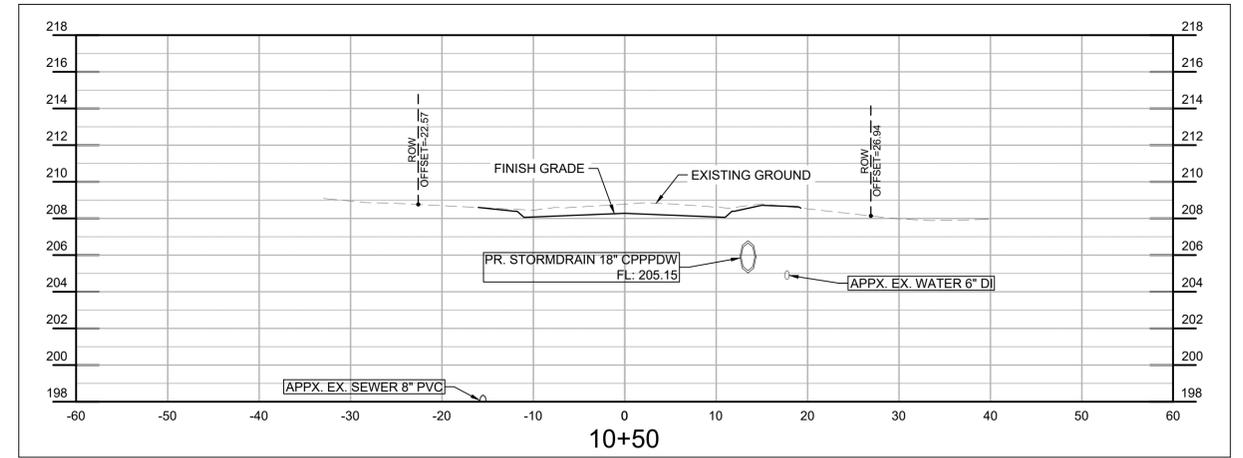
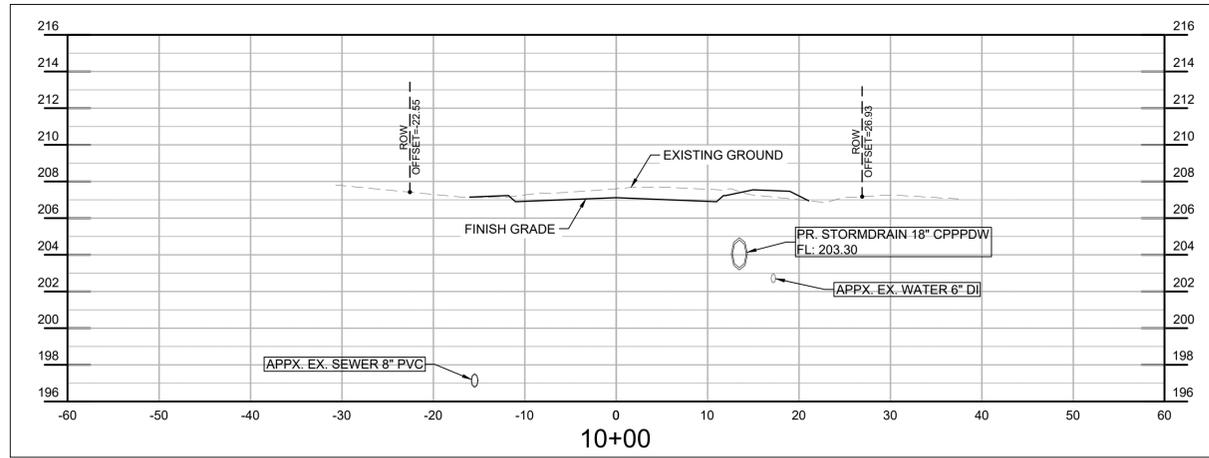
2/17/2026



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/26
SCALE: 1" = 10'
SHEET TITLE
CROSS SECTIONS
(3 OF 7)
C9.2

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REVISION NO.	DATE	DESCRIPTION

DESIGNED BY: JKR
 DRAWN BY: JKR
 CHECKED BY: JB



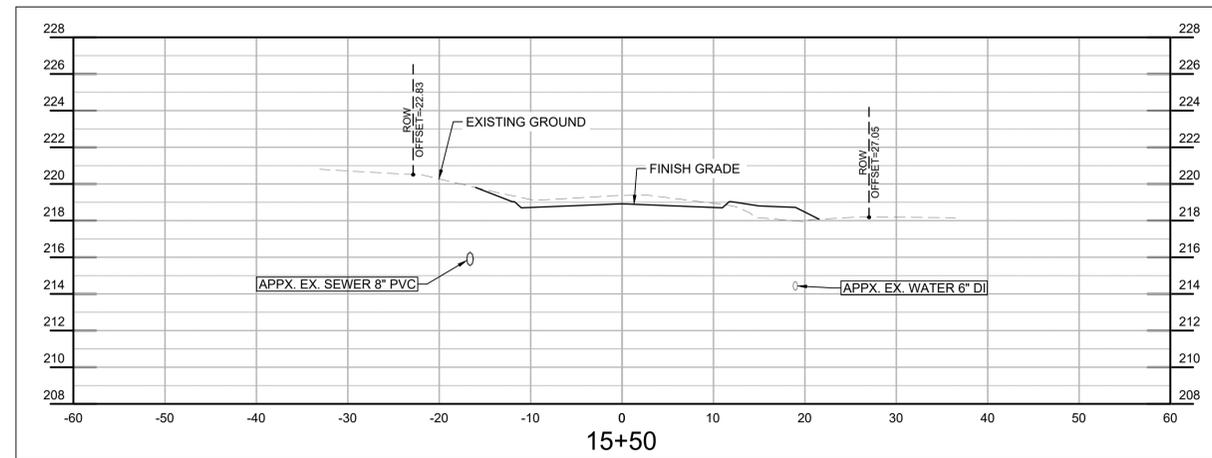
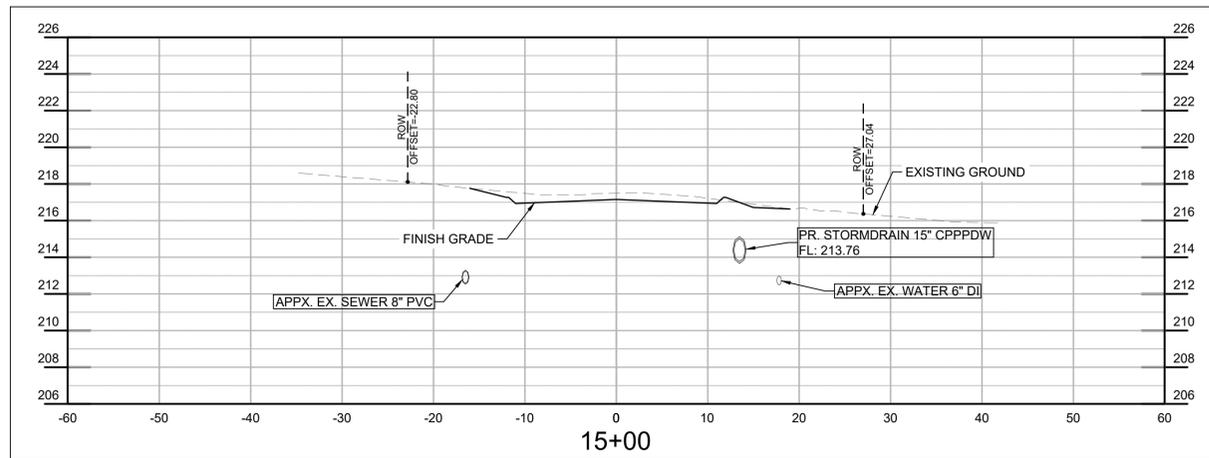
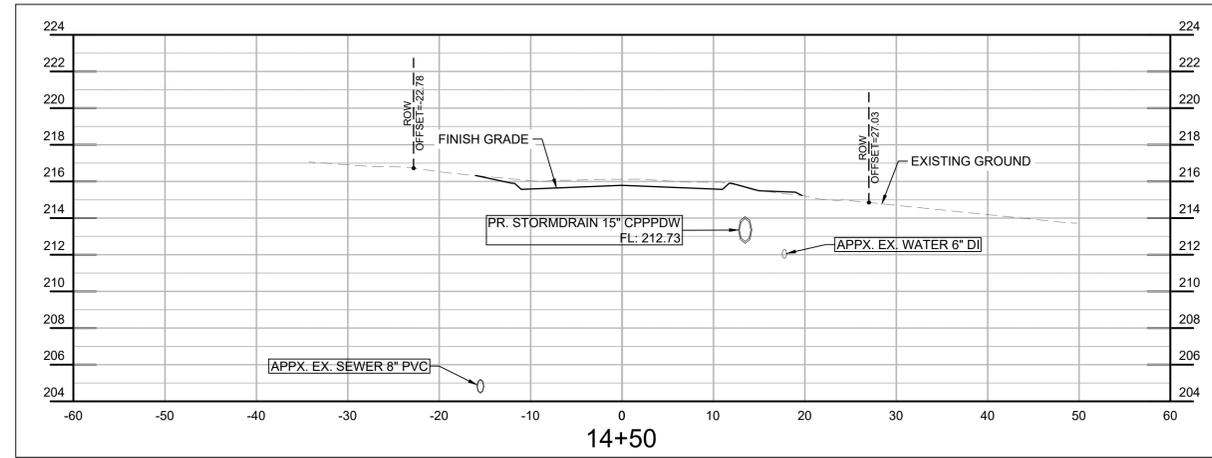
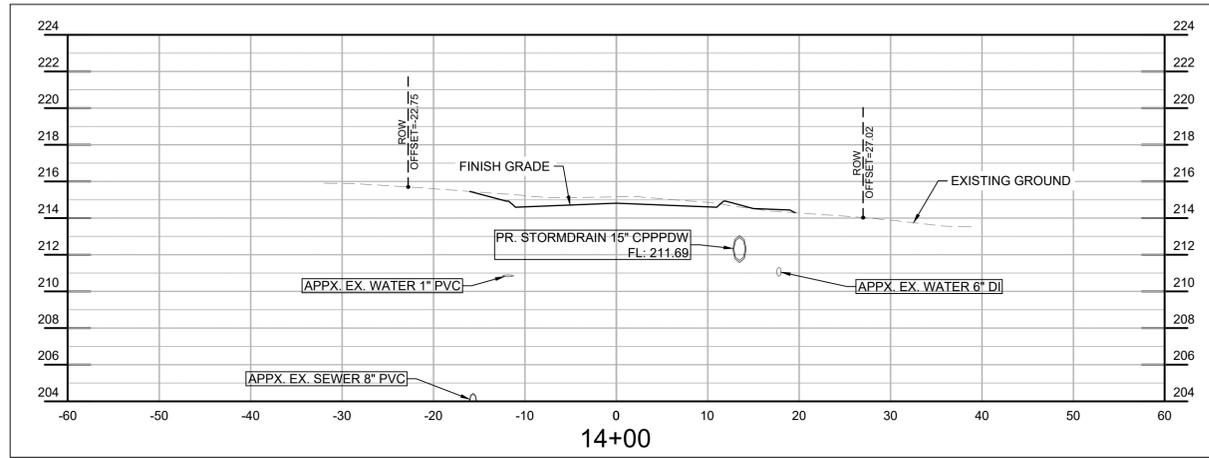
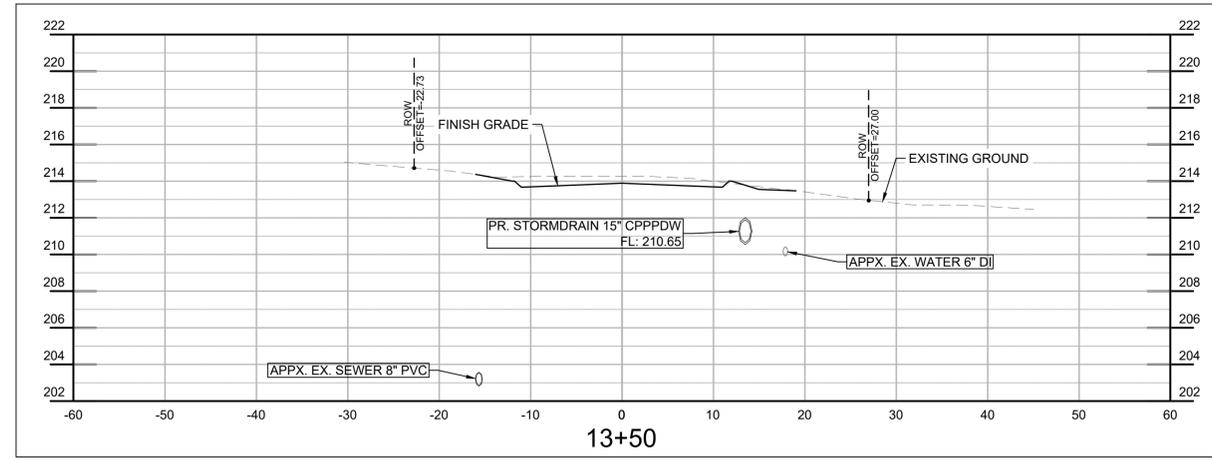
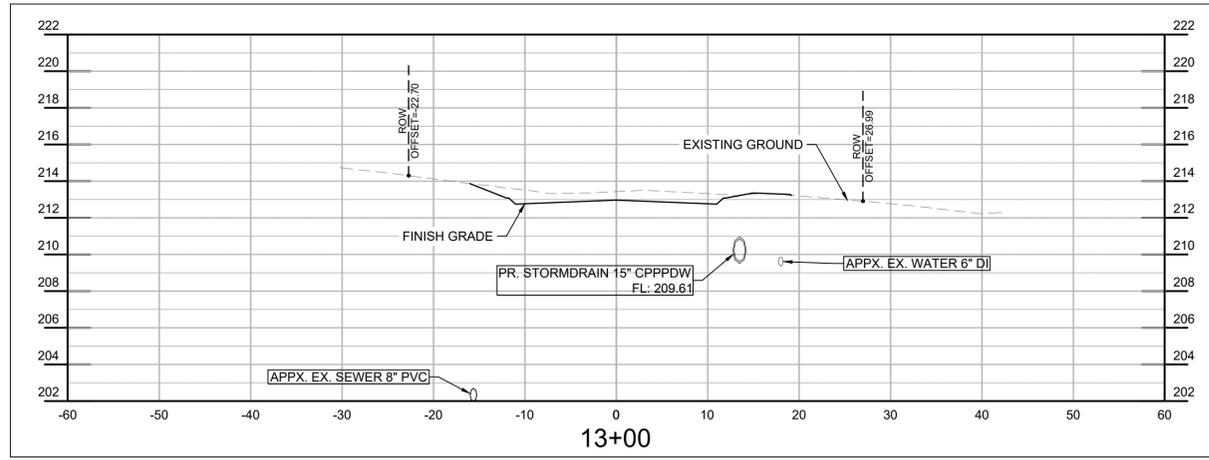
2/17/2026



SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
 ISSUED: 2/17/26
 SCALE: 1" = 10'
 SHEET TITLE
 CROSS SECTIONS
 (4 OF 7)
 C9.3

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REVISION NO.	DATE	DESCRIPTION

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 DESIGNED BY:
 JKR
 DRAWN BY:
 JB
 CHECKED BY:



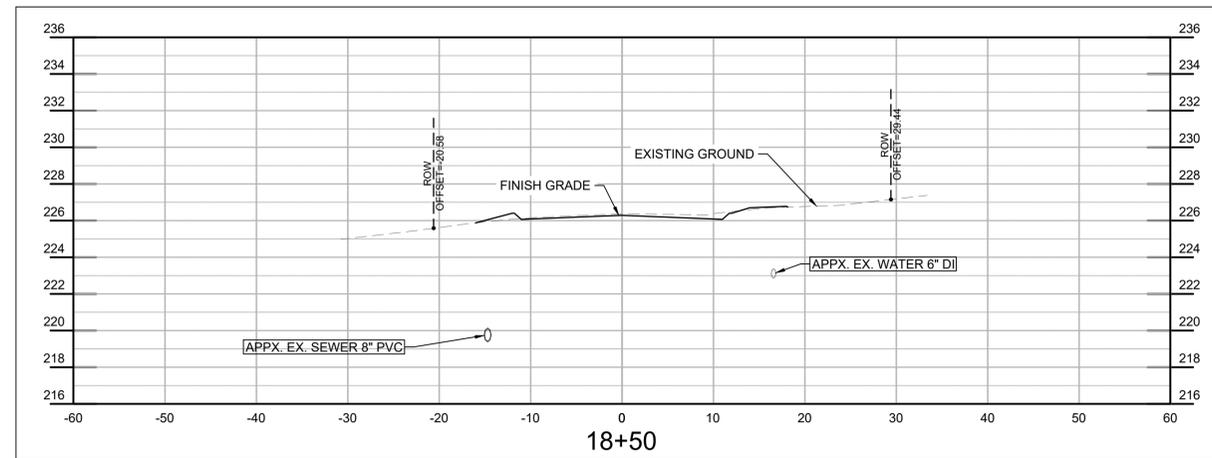
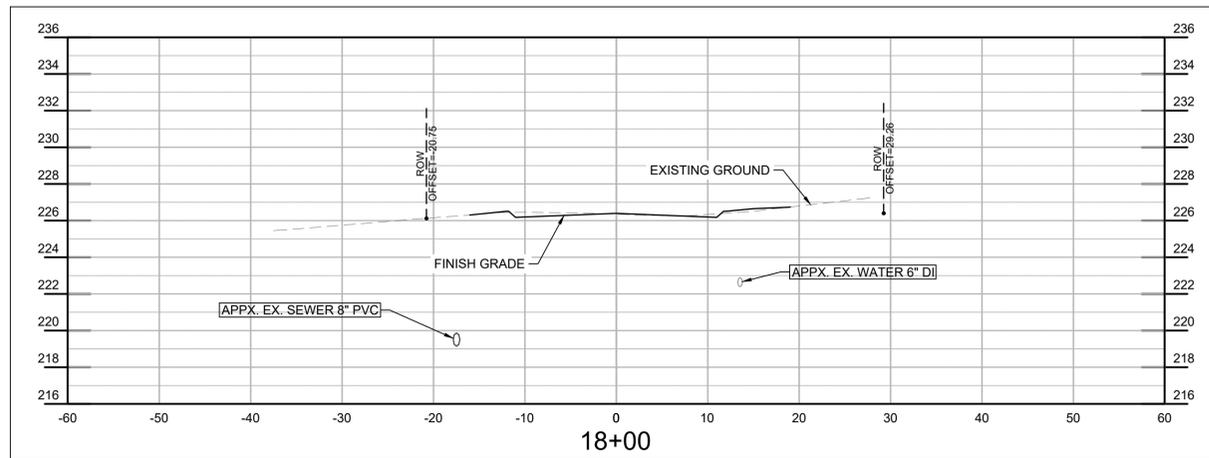
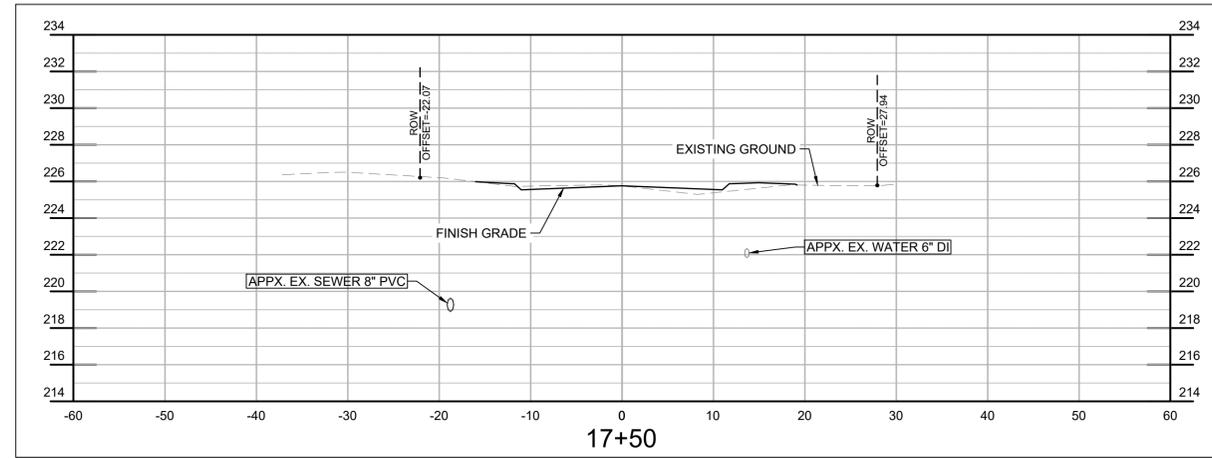
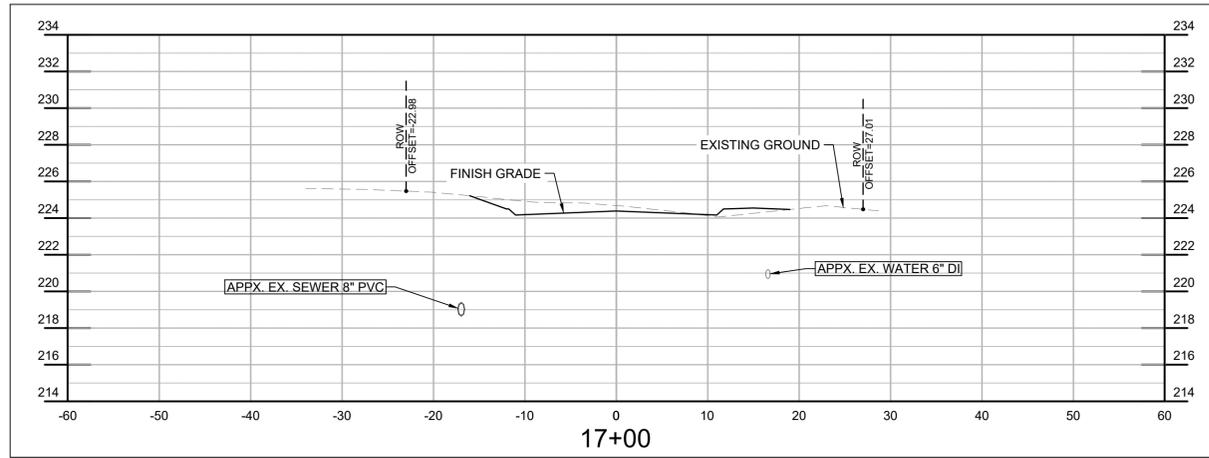
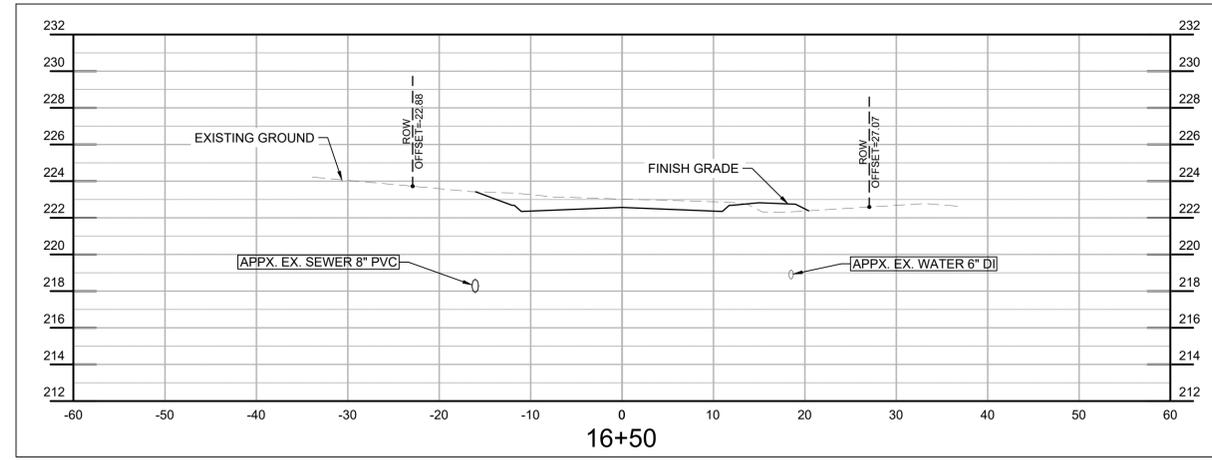
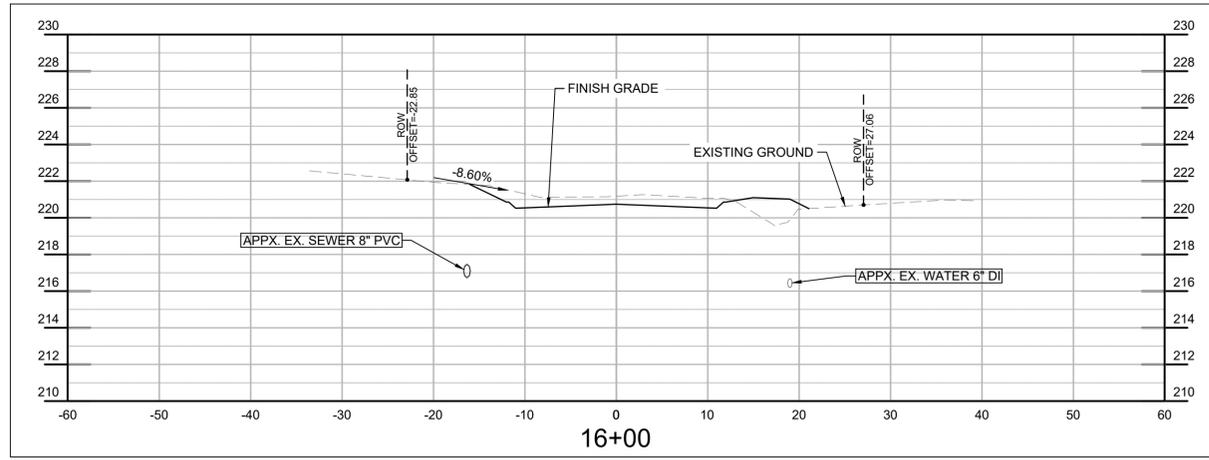
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SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

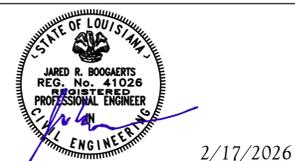
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 (5 OF 7)
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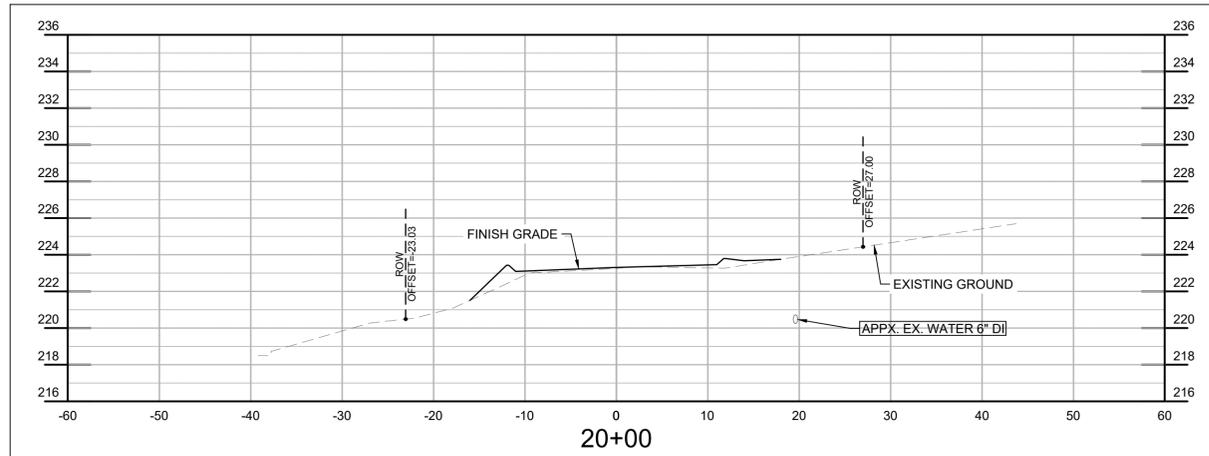
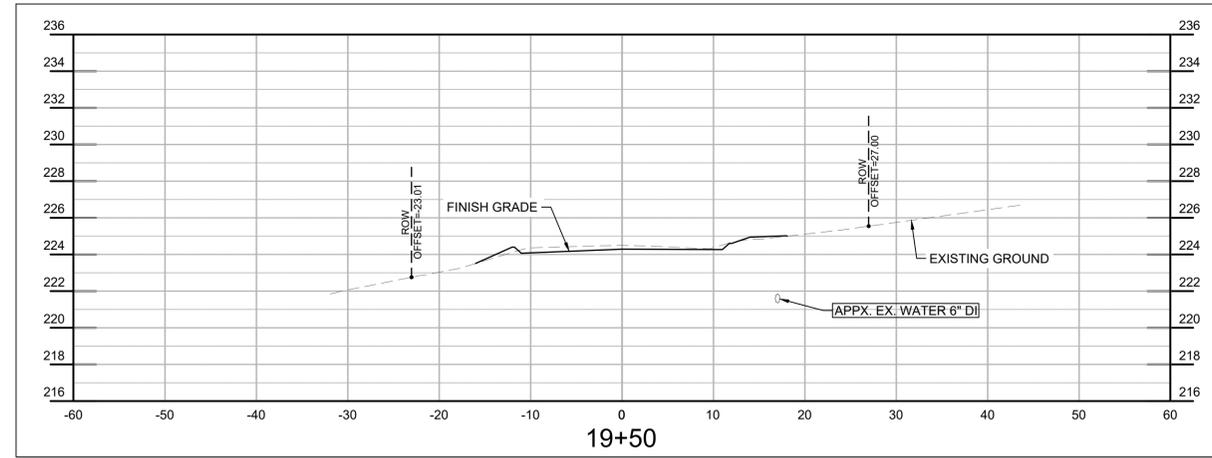
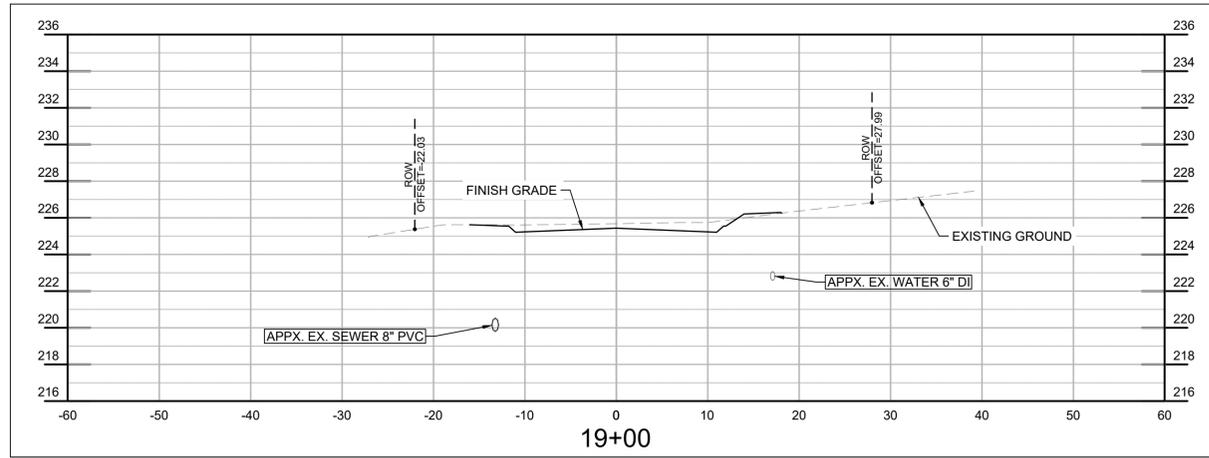
JKR
 DESIGNED BY:
 JKR
 DRAWN BY:
 JB
 CHECKED BY:



SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

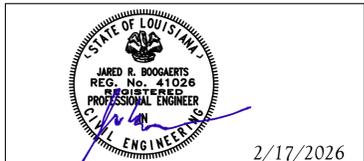
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 (6 OF 7)
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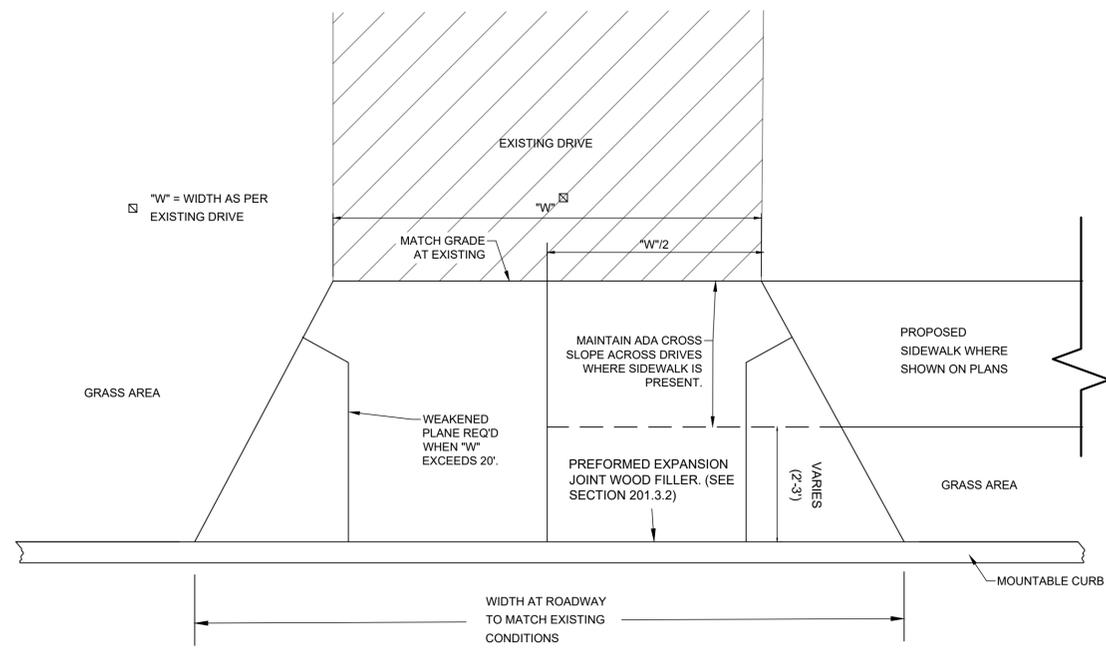
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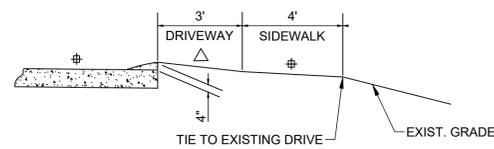
SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
 ISSUED: 2/17/26
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 (7 OF 7)
 C9.6

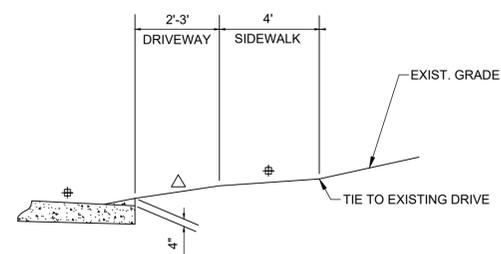
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PLAN OF DRIVEWAY



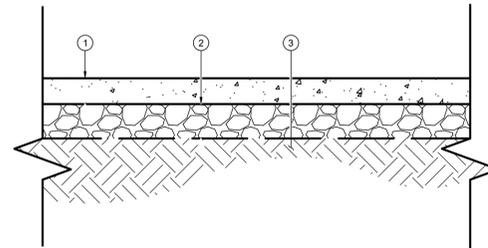
TYPICAL DRIVEWAY IN FILL



TYPICAL DRIVEWAY IN CUT

NOTES:

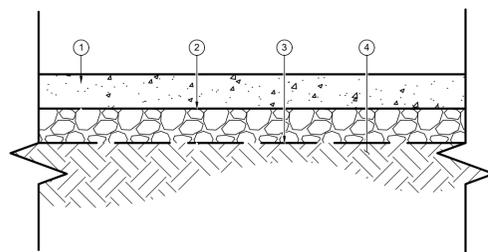
1. DRIVEWAY PROFILES SHOWN ASSUME A SIDEWALK. NOT ALL DRIVEWAYS WILL HAVE SIDEWALKS.
- △ 2. MAXIMUM DRIVEWAY GRADE SHALL BE 20% (25% FOR SPECIAL CASES). MAXIMUM BREAK IN GRADE WITHOUT A VERTICAL CURVE SHALL BE 10% FOR CRESTS AND 9% FOR SAGS, AT NOT LESS THAN 10' INTERVALS.
- ⊕ 3. MAXIMUM SIDEWALK SLOPE SHALL BE 2%. ROADWAY SLOPE SHALL BE AS SHOWN IN PLANS



1. PROPOSED 6" PORTLAND CEMENT CONCRETE REPLACEMENT (TYPE B, MINIMUM COMPRESSION STRENGTH 4000 PSI AFTER 28 DAY CURING)
2. PROPOSED 8" CRUSHED AGGREGATE BASE COURSE (CRUSHED STONE OR GRAVEL) COMPACTED TO 95% STANDARD PROCTOR)
3. EXISTING SUBGRADE (SCARIFIED AND COMPACTED 6" DEPTH TO 95% STANDARD PROCTOR)

TYPICAL DRIVEWAY SECTION

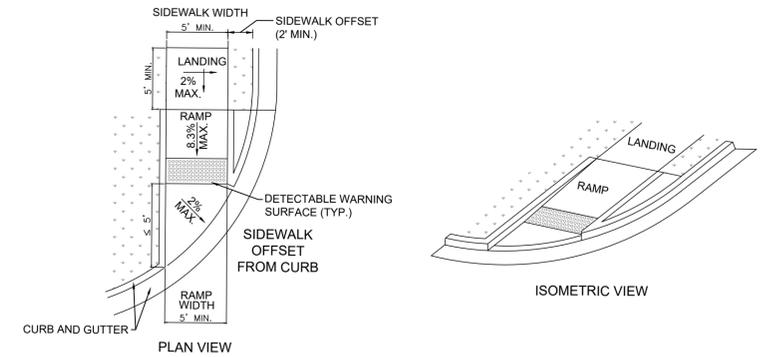
NTS



1. PROPOSED 8" PORTLAND CEMENT CONCRETE REPLACEMENT (TYPE B, MINIMUM COMPRESSION STRENGTH 4000 PSI AFTER 28 DAY CURING)
2. PROPOSED 3" CRUSHED AGGREGATE BASE COURSE (CRUSHED STONE OR GRAVEL) COMPACTED TO 95% STANDARD PROCTOR)
3. GEOTEXTILE FABRIC PER SPECIFICATION
4. EXISTING SUBGRADE (SCARIFIED AND COMPACTED 6" DEPTH TO 95% STANDARD PROCTOR)

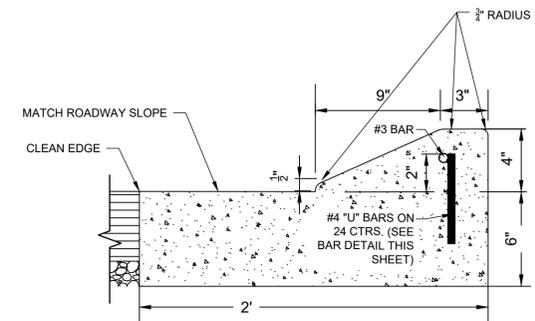
TYPICAL PAVEMENT SECTION

NTS



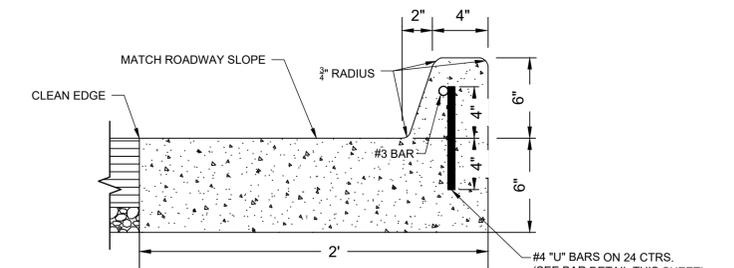
TYPE 7

DIRECTIONAL CURB RAMPS



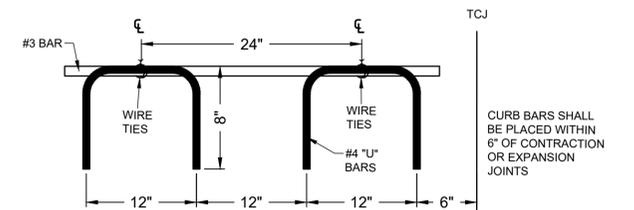
CURB & GUTTER (MOUNTABLE)

N.T.S.



CURB & GUTTER (BARRIER)

N.T.S.



BAR DETAIL

N.T.S.

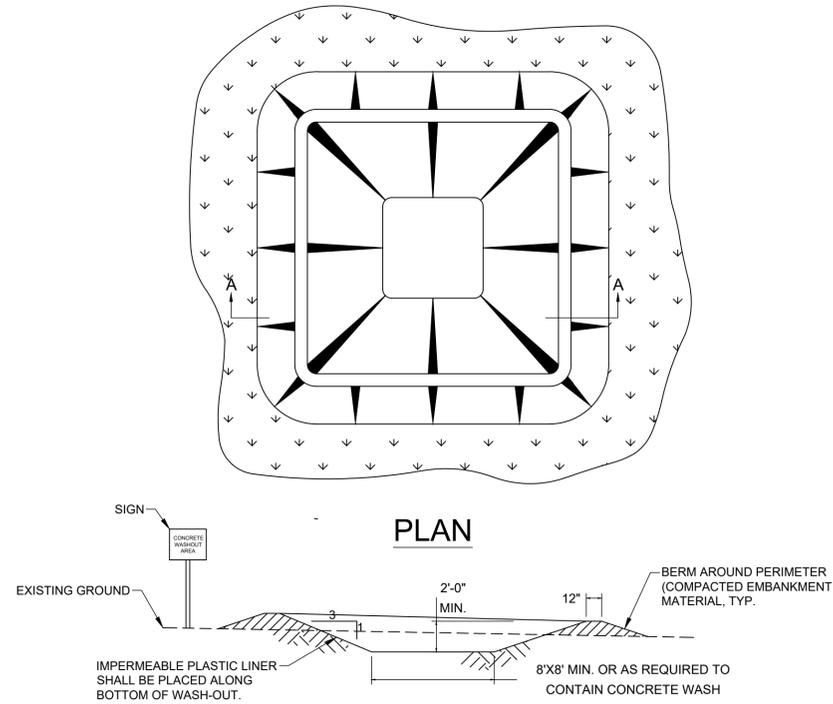
REVISION NO.	DATE	DESCRIPTION

DESIGNED BY: JB
 DRAWN BY: JG
 CHECKED BY: JB



SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

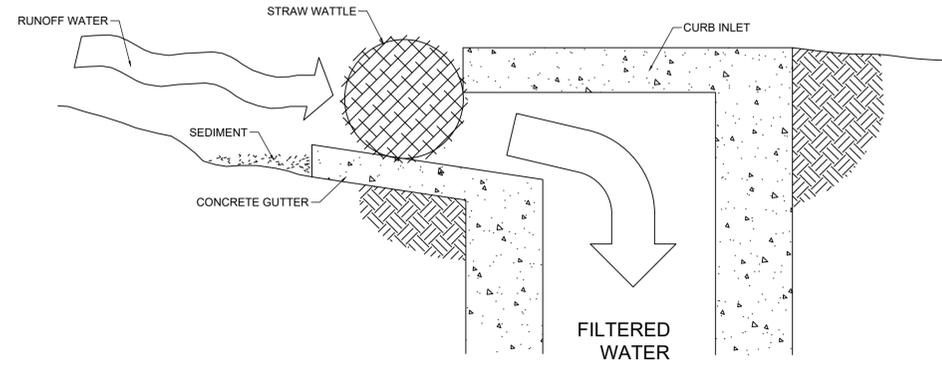
PROJECT NO.: 47786.038
 ISSUED: 2/17/26
 SCALE: N.T.S.
 SHEET TITLE: SITE DETAILS (1 OF 2)
 C10.0



SECTION A-A
CONCRETE WASHOUT AREA
 Not to Scale

NOTES:

1. CONCRETE WASHOUT AREA SHALL BE INSTALLED PRIOR TO ANY CONCRETE PLACEMENT ON SITE.
2. VEHICLE TRACKING CONTROL IS REQUIRED AT CONCRETE WASHOUT ENTRANCE IF ACCESS TO CONCRETE WASHOUT AREA IS OFF PAVEMENT.
3. THE CONCRETE WASHOUT AREA SHALL BE REPAIRED AND/OR ENLARGED AS NECESSARY TO MAINTAIN CAPACITY OR WASTE CONCRETE.
4. WASTE MATERIAL FROM CONCRETE WASHOUT OPERATIONS MUST BE REMOVED AND LEGALLY DISPOSED OF WHEN IT HAS ACCUMULATED TWO-THIRDS OF THE WET STORAGE CAPACITY OF THE STRUCTURE.
5. AT THE END OF CONSTRUCTION, ALL CONCRETE SHALL BE REMOVED FROM THE SITE AND LEGALLY DISPOSED OF AT AN APPROVED WASTE SITE.
6. WHEN THE CONCRETE WASHOUT AREA IS REMOVED, THE DISTURBED AREA SHALL BE SEEDED AND MULCHED OR OTHERWISE STABILIZED.



TEMPORARY STRAW WATTLE
SEDIMENT FILTER
 Not to Scale

FILE NAME: A:\4700\47786\038\PIV\CADD\Sheets\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2026, TIME: 5:05 PM, USER: ah5754

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2/17/2026

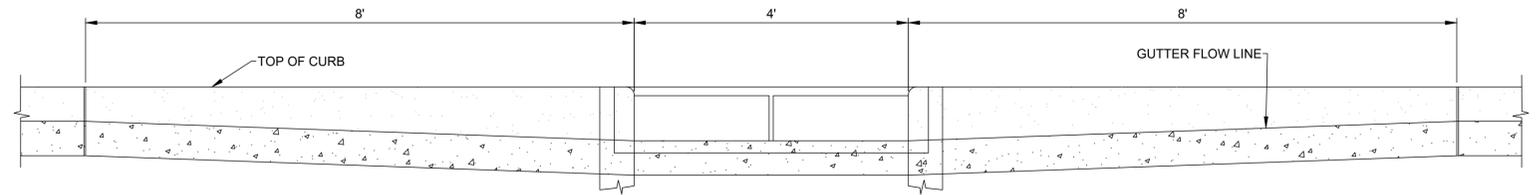


SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

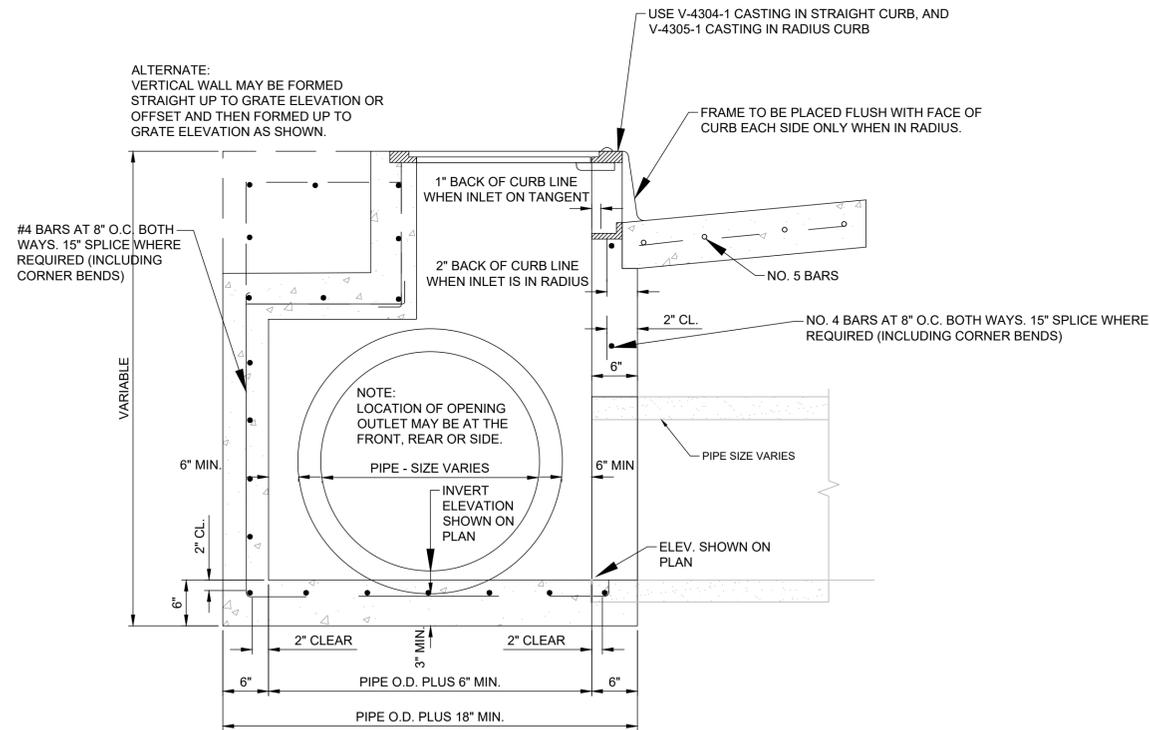
PROJECT NO.:	47786.038
ISSUED:	2/17/26
SCALE:	N.T.S.
SHEET TITLE:	EROSION CONTROL DETAILS C10.2

GENERAL NOTES

1. ALL CORNERS TO BE CHAMFERED 3/4".
2. RUBBED FINISH REQUIRED ON EXPOSED SURFACES.
3. REINFORCED CONCRETE SHALL BE CLASS "A" CONCRETE (3800 PSI - 28 DAYS). PORTLAND CEMENT SHALL CONFORM TO AASHTO DESIGNATION M-85. REINFORCED BARS SHALL CONFORM TO ASTM DESIGNATION A-615 (GRADE 40).
4. STEEL IN FRAME AND COVER TO CONFORM TO ASTM DESIGNATION A-36 AS AMENDED TO DATE, AND SHALL BE GALVANIZED AFTER FABRICATION. GALVANIZING TO CONFORM TO ASTM A-123
5. STEEL MESH TO CONFORM TO ASTM A-185.
6. BACKFILL TO BE COMPACTED AT LEAST 95% DENSITY (STANDARD EFFORT).
7. INLET MAY BE CONSTRUCTED AS SINGLE UNIT OR IN MULTIPLES OF 2 AND 3 UNITS USING THE CONNECTING BEAM.
8. THE MINIMUM DISTANCE FROM THE TOP OF A PIPE ENTERING OR LEAVING THE INLET BOX SHALL BE 2'-6" FOR PIPE UNDER PAVEMENT (FRONT OF INLET BOX) AND 2' FOR SIDES AND BACK OF THE INLET BOX.
9. STORM SEWER PIPE SHOULD BE INSTALLED BEFORE INTAKE SIDEWALL CONSTRUCTION IS STARTED. SIDEWALLS SHOULD BE CONSTRUCTED AS INDICATED WITH OPENINGS FOR STORM SEWER SMOOTHLY SHAPED AND NO INLET PIPES PROJECTING UNNECESSARILY INTO WALL. OUTLET PIPE SHALL NOT PROJECT BEYOND INSIDE FACE OF SIDEWALL.
10. ALL REINFORCING STEEL SHALL BE 1-1/2" CLEAR OF CONCRETE SURFACE UNLESS OTHERWISE NOTED.
11. REINFORCING STEEL SHALL BE BENT AROUND PIPE OPENING WHEN POSSIBLE. IF STEEL IS CUT, A DIAGONAL BAR SHALL BE USED TO TIE ALL CUT END TOGETHER.
12. EXPANSION JOINT DETAIL AND DOWEL PLACEMENT SAME AS THAT SHOWN APPROPRIATE PAVEMENT. AND CURB STANDARD PLANS.
13. A CONCRETE FILLET SHALL BE PLACED IN THE BOTTOM OF THE INTAKE APPROXIMATELY AS INDICATED OR DIRECTED BY THE CITY ENGINEER. SPECIAL SHAPING OF THIS FILLET IS REQUIRED TO PROVIDE A SMOOTH CHANNEL THROUGH THE INLET BOX. TOP SURFACE OF THE FILLET SHALL SLOPE APPROXIMATELY 1 INCH PER FOOT TOWARD THE CHANNEL.
14. NO SCALE, FOLLOW DIMENSIONS.



INLET TRANSITION DETAIL
N.T.S.
(SHOWING MANNER OF PLACING)



INLET SECTION B-B
N.T.S.

FILE NAME: A:\47000\47786\038\DWG\CADD\Sheets\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2026, TIME: 5:05 PM, USER: ah5754

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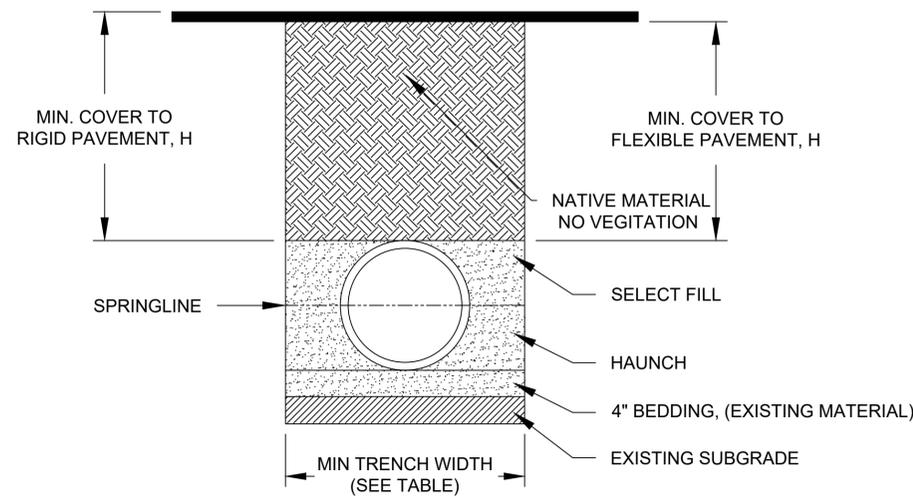
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JB
DRAWN BY:
JG
CHECKED BY:
JB



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/26
SCALE: N.T.S.
SHEET TITLE
STORM DRAIN
DETAILS (1 OF 5)
C10.3

CPPPDW STORMDRAIN TRENCH INSTALLATION DETAIL



NOTES:

1. ALL PIPE SYSTEMS SHALL BE INSTALLED IN ACCORDANCE WITH ASTM D2321, "STANDARD PRACTICE FOR UNDERGROUND INSTALLATION OF THERMOPLASTIC PIPE FOR SEWERS AND OTHER GRAVITY FLOW APPLICATIONS", LATEST EDITION, WITH THE EXCEPTION THAT THE INITIAL BACKFILL MAY EXTEND TO THE CROWN OF THE PIPE. SOIL CLASSIFICATIONS ARE PER THE LATEST VERSION OF ASTM D2321. CLASS IVB MATERIALS (MH, CH) AS DEFINED IN PREVIOUS VERSIONS OF ASTM D2321 ARE NOT APPROPRIATE BACKFILL MATERIALS.
2. MEASURES SHOULD BE TAKEN TO PREVENT MIGRATION OF NATIVE FINES INTO BACKFILL MATERIAL, WHEN REQUIRED.
3. **FOUNDATION:** WHERE THE TRENCH BOTTOM IS UNSTABLE, THE CONTRACTOR SHALL EXCAVATE TO A DEPTH REQUIRED BY THE ENGINEER AND REPLACE WITH SUITABLE MATERIAL AS SPECIFIED BY THE ENGINEER. AS AN ALTERNATIVE AND AT THE DISCRETION OF THE DESIGN ENGINEER, THE TRENCH BOTTOM MAY BE STABILIZED USING A GEOTEXTILE MATERIAL.
4. **BEDDING:** SUITABLE MATERIAL SHALL BE CLASS I, II, OR III. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER. COMPACTION SHALL BE IN ACCORDANCE WITH TABLE 3 FOR THE APPLICABLE FILL HEIGHTS LISTED. UNLESS OTHERWISE NOTED BY THE ENGINEER, MINIMUM BEDDING THICKNESS SHALL BE 4" (100mm) FOR 12"-24" (300mm-600mm) DIAMETER PIPE; 6" (150mm) FOR 30"-60" (750mm-1500mm) DIAMETER PIPE. THE MIDDLE 1/3 BENEATH THE PIPE INVERT SHALL BE LOOSELY PLACED.
5. **INITIAL BACKFILL:** SUITABLE MATERIAL SHALL BE CLASS I, II, III, OR IV IN THE PIPE ZONE EXTENDING TO THE CROWN OF THE PIPE. THE CONTRACTOR SHALL PROVIDE DOCUMENTATION FOR MATERIAL SPECIFICATION TO ENGINEER. MATERIAL SHALL BE INSTALLED AS REQUIRED IN ASTM D2321, LATEST EDITION. COMPACTION SHALL BE IN ACCORDANCE WITH TABLE 3 FOR THE APPLICABLE FILL HEIGHTS LISTED.
6. **MINIMUM COVER:** MINIMUM COVER, H, IN NON-TRAFFIC APPLICATIONS (GRASS OR LANDSCAPE AREAS) IS 12" (300mm) FROM THE TOP OF PIPE TO GROUND SURFACE. ADDITIONAL COVER MAY BE REQUIRED TO PREVENT FLOTATION. FOR TRAFFIC APPLICATIONS; CLASS I OR II MATERIAL COMPACTED TO 90% SPD AND CLASS III COMPACTED TO 95% SPD IS REQUIRED. FOR TRAFFIC APPLICATIONS, MINIMUM COVER, H, IS 12" (300mm) UP TO 48" (1200mm) DIAMETER PIPE AND 24" (600mm) OF COVER FOR 60" (1500mm) DIAMETER PIPE, MEASURED FROM TOP OF PIPE TO BOTTOM OF FLEXIBLE PAVEMENT OR TO TOP OF RIGID PAVEMENT.

TABLE 1, RECOMMENDED MINIMUM TRENCH WIDTHS

PIPE DIAM.	MIN. TRENCH WIDTH
12" (300mm)	30" (762mm)
15" (375mm)	34" (864mm)
18" (450mm)	39" (991mm)
24" (600mm)	48" (1219mm)
30" (750mm)	56" (1422mm)
36" (900mm)	64" (1626mm)
42" (1050mm)	72" (1829mm)
48" (1200mm)	80" (2032mm)
60" (1500mm)	96" (2438mm)

TABLE 2, MINIMUM RECOMMENDED COVER BASED ON VEHICLE LOADING CONDITIONS

PIPE DIAM.	SURFACE LIVE LOADING CONDITION	
	H-25	HEAVY CONSTRUCTION (75T AXLE LOAD) *
12" - 48" (300mm - 1200mm)	12" (305mm)	48" (1219mm)
60" (1500mm)	24" (610mm)	60" (1524mm)

* VEHICLES IN EXCESS OF 75T MAY REQUIRE ADDITIONAL COVER

TABLE 3, MAXIMUM COVER FOR ADS HP STORM PIPE, ft

PIPE DIA	CLASS I		CLASS II		CLASS III		CLASS IV
	COMPACTED	95%	90%	85%	95%	90%	95%
12" (300mm)	41 (12.5m)	28 (8.5m)	21 (6.4m)	16 (4.9m)	20 (6.1m)	16 (4.9m)	16 (4.9m)
15" (375mm)	42 (12.8m)	29 (8.8m)	21 (6.4m)	16 (4.9m)	21 (6.4m)	16 (4.9m)	16 (4.9m)
18" (450mm)	44 (13.4m)	30 (9.1m)	21 (6.4m)	16 (4.9m)	22 (6.7m)	17 (5.2m)	16 (4.9m)
24" (600mm)	30 (9.1m)	21 (6.4m)	15 (4.6m)	11 (3.4m)	16 (4.9m)	11 (3.4m)	11 (3.4m)
30" (750mm)	39 (11.9m)	27 (8.2m)	19 (5.8m)	14 (4.3m)	19 (5.8m)	15 (4.6m)	14 (4.3m)
36" (900mm)	28 (8.5m)	20 (6.1m)	14 (4.3m)	10 (3.0m)	14 (4.3m)	11 (3.4m)	10 (3.0m)
42" (1050mm)	30 (9.1m)	21 (6.4m)	14 (4.3m)	10 (3.0m)	15 (4.6m)	11 (3.4m)	10 (3.0m)
48" (1200mm)	29 (8.8m)	20 (6.1m)	14 (4.3m)	9 (2.7m)	14 (4.3m)	10 (3.0m)	10 (3.0m)
60" (1500mm)	29 (8.8m)	20 (6.1m)	14 (4.3m)	9 (2.7m)	14 (4.3m)	10 (3.0m)	9 (2.7m)

FILL HEIGHT TABLE GENERATED USING AASHTO SECTION 12, LOAD RESISTANCE FACTOR DESIGN (LRFD) PROCEDURE WITH THE FOLLOWING ASSUMPTIONS:

NO HYDROSTATIC PRESSURE
UNIT WEIGHT OF SOIL (γ_s) = 120 PCF

FILE NAME: A:\4700\47786\038\PW\CADD\Sheets\C-PLAN-C700-DTLS-47786-038.dwg DATE: February 17, 2026, TIME: 5:05 PM, USER: ah5754

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DESIGNED BY: JB
DRAWN BY: JG
CHECKED BY: JB



401 MARKET ST.
SUITE 650
SHREVEPORT, LA 71101
TEL. (318) 716-6136



JARED R. BOOGAERTS
REG. No. 41026
REGISTERED PROFESSIONAL ENGINEER
CIVIL ENGINEERING

2/17/2026

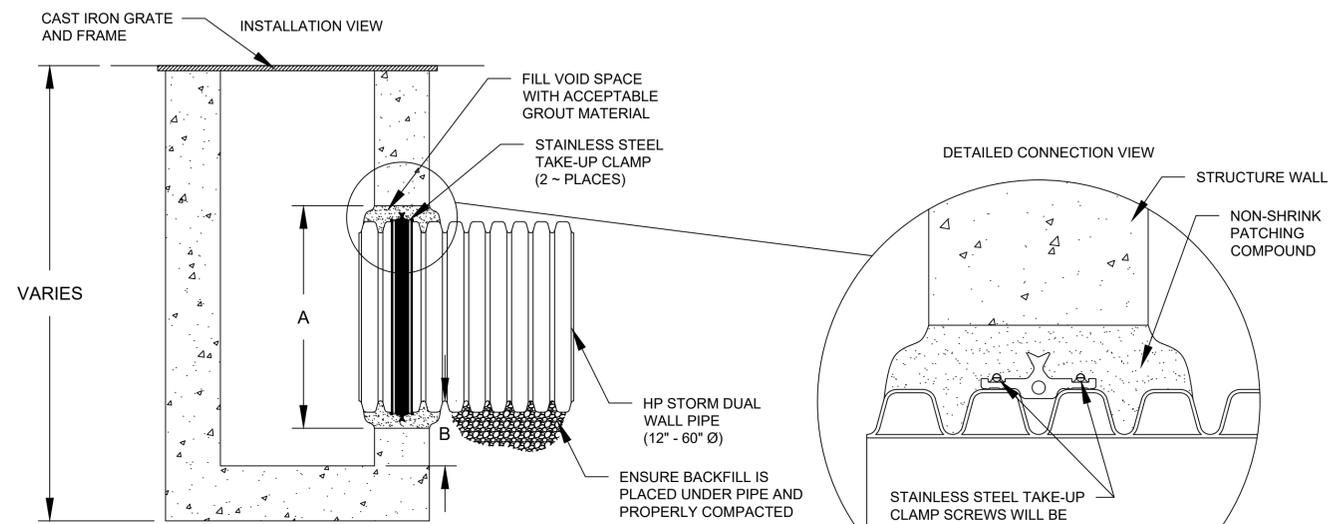


SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/26
SCALE: N.T.S.
SHEET TITLE
STORM DRAIN
DETAILS (2 OF 5)
C10.4

12"-60" CPPPDW WATERSTOP GROUDED CONNECTION

INSERTA TEE® CONNECTION DETAILS

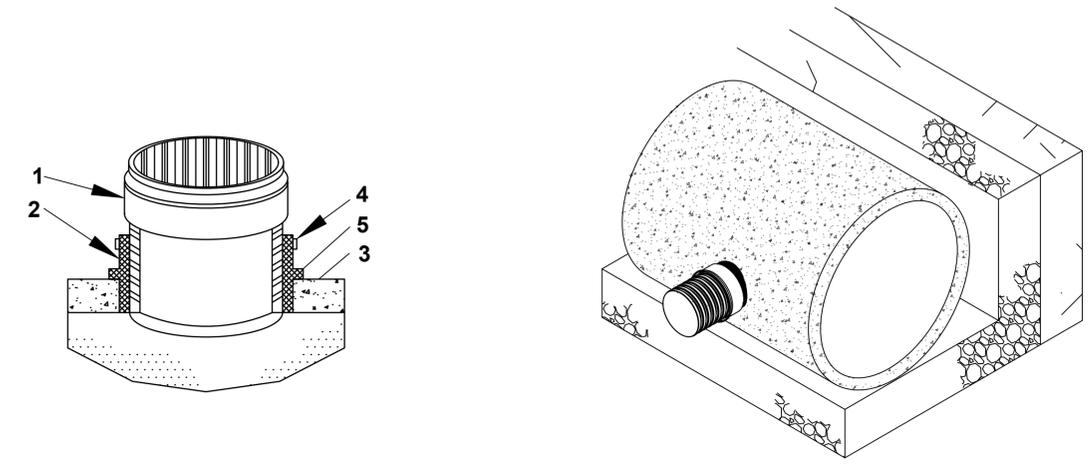


PIPE SIZE	PIPE OD	"A" MIN. HOLE Ø	"B" MIN. DISTANCE FROM PIPE INVERT TO STRUCTURE INVERT
12" (300mm)	14.5" (368mm)	19.50" (495mm)	3.7" (94mm)
15" (375mm)	17.6" (447mm)	23.00" (584mm)	4.0" (102mm)
18" (450mm)	21.2" (538mm)	26.50" (673mm)	4.2" (107mm)
21" (525mm)	24.8" (630mm)	30.25" (768mm)	4.5" (114mm)
24" (600mm)	27.8" (706mm)	33.25" (845mm)	4.5" (114mm)
30" (750mm)	35.1" (892mm)	40.50" (1029mm)	5.2" (132mm)
36" (900mm)	41.1" (1044mm)	47.00" (1194mm)	5.5" (140mm)
42" (1050mm)	47.7" (1212mm)	53.00" (1346mm)	5.7" (145mm)
48" (1200mm)	53.6" (1361mm)	59.00" (1499mm)	5.7" (145mm)
60" (1500mm)	66.3" (1684mm)	72.00" (1829mm)	6.4" (163mm)

NOTES:

PERFORMANCE HIGHLY DEPENDENT ON INSTALLATION. CONTRACTOR MUST ENSURE MANHOLE GASKET IS UNIFORMLY SEATED AROUND STRUCTURE ADAPTER. EXTRA PRECAUTIONS MUST BE TAKEN TO PREVENT DIFFERENTIAL SETTLEMENT BETWEEN THE PIPE AND MANHOLE.

WATERSTOP INSTALLATION REQUIRED BY CITY OF SHREVEPORT.



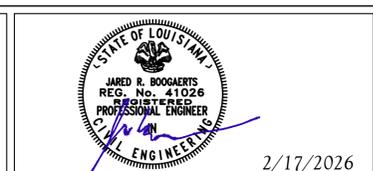
PART #	DESCRIPTION
1	PVC hub adapter which is driven into the center of the rubber sleeve after the rubber sleeve is in the hole.
2	Complete rubber sleeve consisting of ASTM F 477 specifications.
3	Mainline pipe wall where branch line is connected.
4	Stainless steel band, put on above rubber segment as an added precaution.
5	Rubber segment which is molded into the rubber sleeve. This prevents the rubber sleeve from going through the drilled hole when PVC hub is being driven into the rubber sleeve.

Note: Watertight seal is accomplished when PVC hub is driven into rubber sleeve causing the rubber sleeve to compress between pipe wall and PVC hub.

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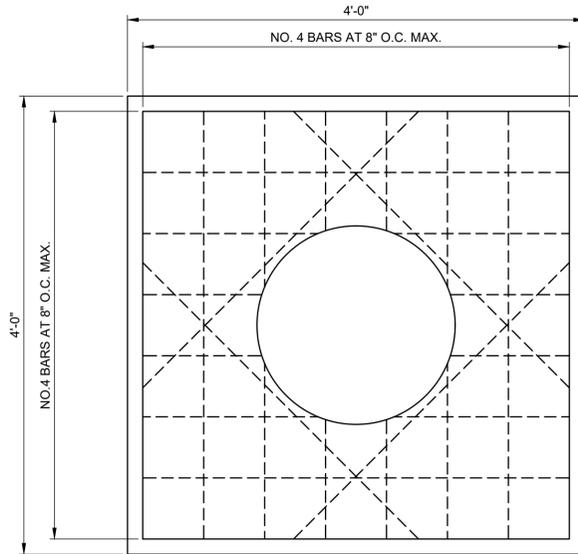


SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

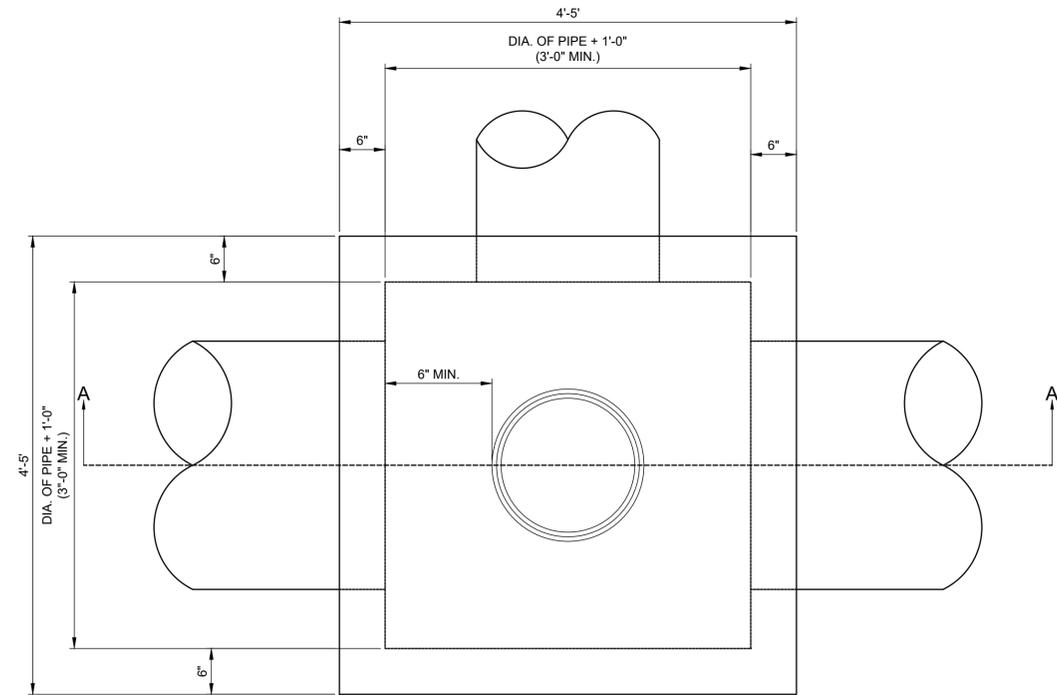
PROJECT NO.: 47786.038
 ISSUED: 2/17/26
 SCALE: N.T.S.
 SHEET TITLE
 STORM DRAIN
 DETAILS (3 OF 5)
 C10.5

JUNCTION BOX NOTES

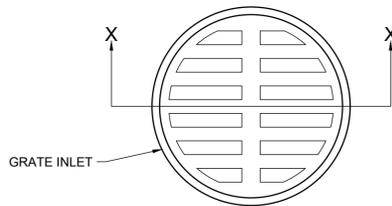
1. ALL CORNERS TO BE CHAMFERED 3/4".
2. RUBBED FINISH REQUIRED ON EXPOSED SURFACES.
3. REINFORCED CONCRETE SHALL BE CLASS "A" CONCRETE (3800 PSI - 28 DAYS). PORTLAND CEMENT SHALL CONFORM TO AASHTO DESIGNATION M-85. REINFORCED BARS SHALL CONFORM TO ASTM DESIGNATION A-615 (GRADE 40).
4. STEEL IN FRAME AND COVER TO CONFORM TO ASTM DESIGNATION A-36 AS AMENDED TO DATE, AND SHALL BE GALVANIZED AFTER FABRICATION. GALVANIZING TO CONFORM TO ASTM A-123.
5. STEEL MESH TO CONFORM TO ASTM A-185.
6. BACKFILL TO BE COMPACTED AT LEAST 95% DENSITY (STANDARD EFFORT).
7. INLET MAY BE CONSTRUCTED AS SINGLE UNIT OR IN MULTIPLES OF 2 AND 3 UNITS USING THE CONNECTING BEAM.
8. THE MINIMUM DISTANCE FROM THE TOP OF A PIPE ENTERING OR LEAVING THE INLET BOX SHALL BE 2'-6". FOR PIPE UNDER PAVEMENT (FRONT OF INLET BOX) AND 2' FOR SIDES AND BACK OF THE INLET BOX.
9. STORM SEWER PIPE SHOULD BE INSTALLED BEFORE INTAKE SIDEWALL CONSTRUCTION IS STARTED. SIDEWALLS SHOULD BE CONSTRUCTED AS INDICATED WITH OPENINGS FOR STORM SEWER SMOOTHLY SHAPED AND NO INLET PIPES PROJECTING UNNECESSARILY INTO WALL. OUTLET PIPE SHALL NOT PROJECT BEYOND INSIDE FACE OF SIDEWALL.
10. ALL REINFORCING STEEL SHALL BE 1-1/2" CLEAR OF CONCRETE SURFACE UNLESS OTHERWISE NOTED.
11. REINFORCING STEEL SHALL BE BENT AROUND PIPE OPENING WHEN POSSIBLE. IF STEEL IS CUT, A DIAGONAL BAR SHALL BE USED TO TIE ALL CUT END TOGETHER.
12. EXPANSION JOINT DETAIL AND DOWEL PLACEMENT SAME AS THAT SHOWN APPROPRIATE PAVEMENT AND CURB STANDARD PLANS.
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14. NO SCALE, FOLLOW DIMENSIONS.



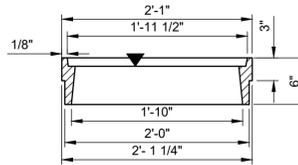
PLAN OF REINFORCING IN TOP
NOT TO SCALE



JUNCTION BOX PLAN
NOT TO SCALE



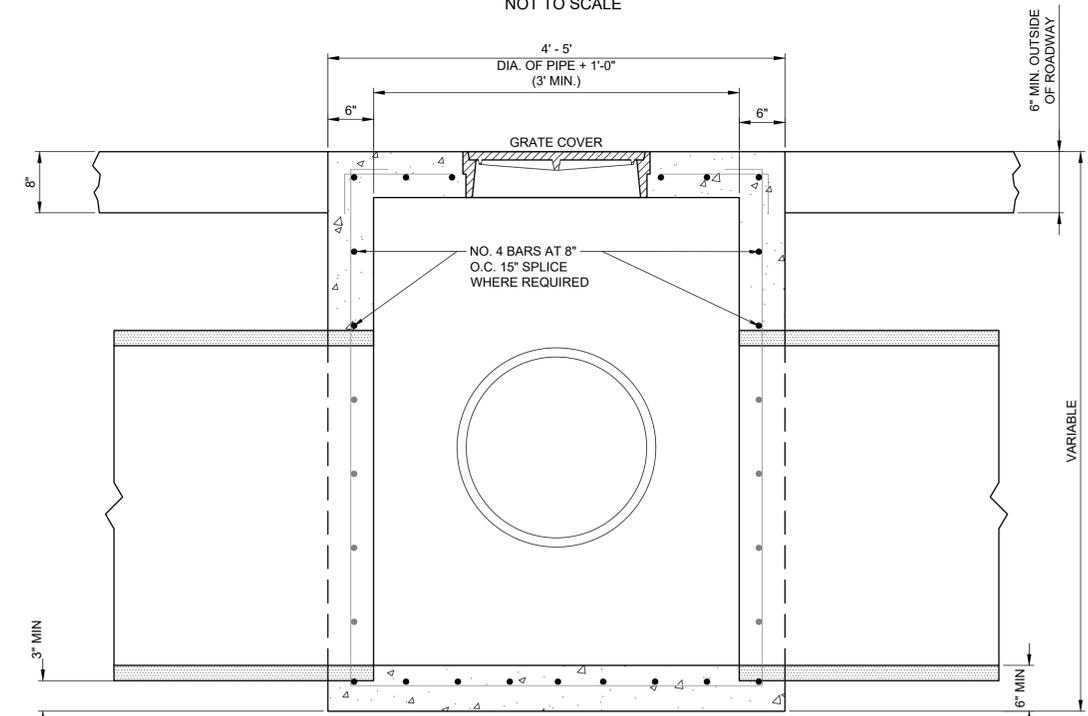
FRAME TOP VIEW



FRAME SECTION "X-X"
DETAILS OF C.I. COVER SEAT
VULCAN V-1866 FRAME OR APPROVED EQUAL
NOT TO SCALE

▼ MACHINED BEARING SURFACE

GRATE COVER
SCALE: N.T.S.



JUNCTION BOX SECTION "A-A"
NOT TO SCALE

JUNCTION BOX
SCALE: N.T.S.

FILE NAME: A:\47000\47786\038\1\PC\CADD\Sheets\C-PLAN-C700-DTLs-47786-038.dwg DATE: February 17, 2026, TIME: 5:05 PM, USER: ah5754

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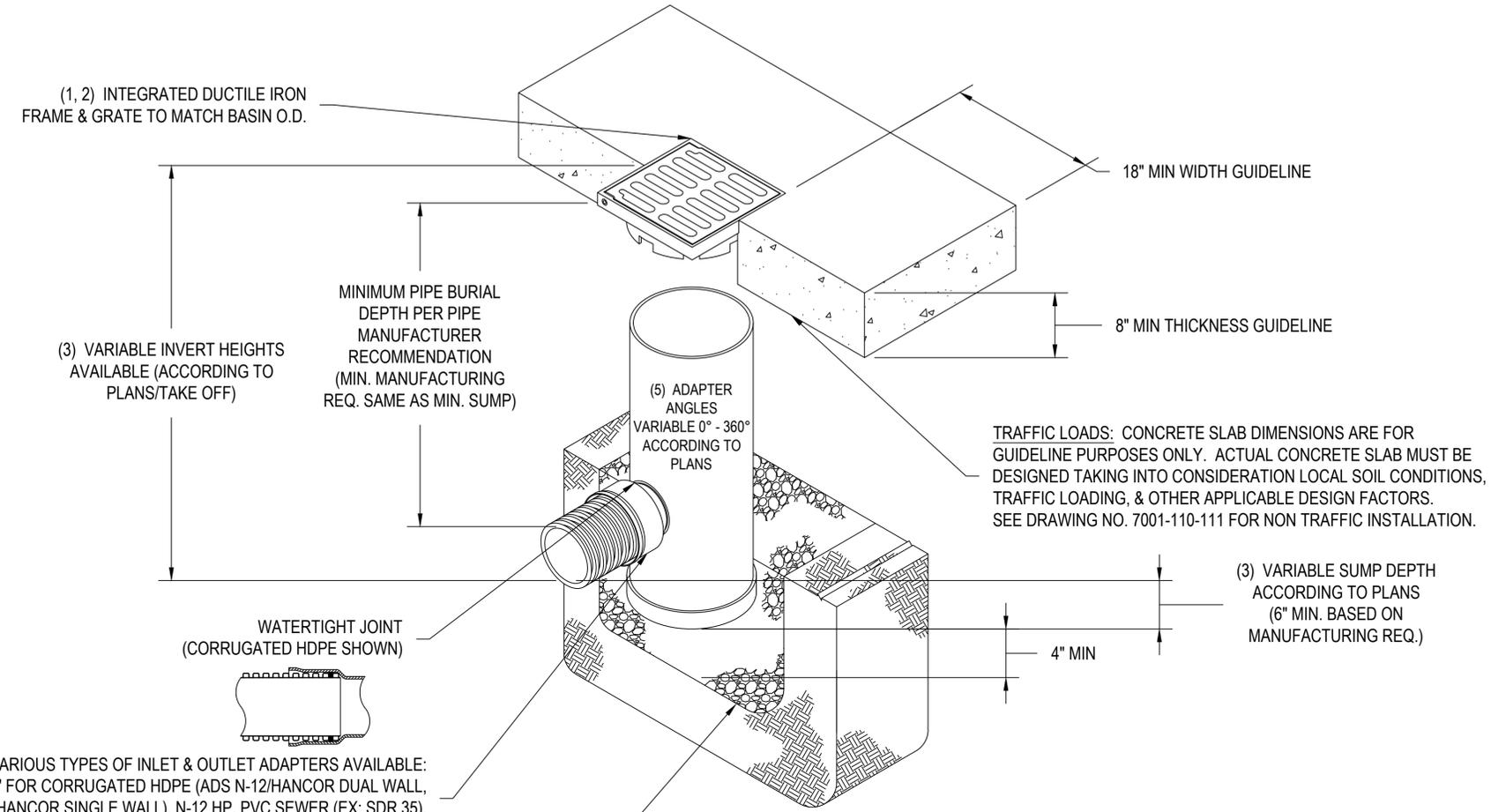
2/17/2026



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
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SCALE: N.T.S.
SHEET TITLE
STORM DRAIN
DETAILS (4 OF 5)
C10.6

NYLOPLAST 12" DRAIN BASIN



(4) VARIOUS TYPES OF INLET & OUTLET ADAPTERS AVAILABLE:
 4" - 12" FOR CORRUGATED HDPE (ADS N-12/HANCOR DUAL WALL,
 ADS/HANCOR SINGLE WALL), N-12 HP, PVC SEWER (EX: SDR 35),
 PVC DWV (EX: SCH 40), PVC C900/C905, CORRUGATED & RIBBED PVC

THE BACKFILL MATERIAL SHALL BE CRUSHED STONE OR OTHER
 GRANULAR MATERIAL MEETING THE REQUIREMENTS OF CLASS I,
 CLASS II, OR CLASS III MATERIAL AS DEFINED IN ASTM D2321.
 BEDDING & BACKFILL FOR SURFACE DRAINAGE INLETS SHALL BE
 PLACED & COMPACTED UNIFORMLY IN ACCORDANCE WITH ASTM D2321.

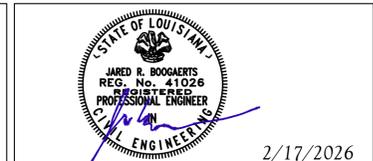
GRATE OPTIONS	LOAD RATING	PART #	DRAWING #
PEDESTRIAN	MEETS H-10	1299CGP	7001-110-202
STANDARD	MEETS H-20	1299CGS	7001-110-203
SOLID COVER	MEETS H-20	1299CGC	7001-110-204
PEDESTRIAN BRONZE	N/A	1299CGPB	7001-110-205
DOME	N/A	1299CGD	7001-110-206
DROP IN GRATE	LIGHT DUTY	1201DI	7001-110-021

- 1 - GRATES/SOLID COVER SHALL BE DUCTILE IRON PER ASTM A536 GRADE 70-50-05, WITH THE EXCEPTION OF THE BRONZE GRATE.
- 2 - FRAMES SHALL BE DUCTILE IRON PER ASTM A536 GRADE 70-50-05
- 3 - DRAIN BASIN TO BE CUSTOM MANUFACTURED ACCORDING TO PLAN DETAILS. RISERS ARE NEEDED FOR BASINS OVER 84" DUE TO SHIPPING RESTRICTIONS. SEE DRAWING NO. 7001-110-065
- 4 - DRAINAGE CONNECTION STUB JOINT TIGHTNESS SHALL CONFORM TO ASTM D3212 FOR CORRUGATED HDPE (ADS N-12/HANCOR DUAL WALL), N-12 HP, & PVC SEWER.
- 5 - ADAPTERS CAN BE MOUNTED ON ANY ANGLE 0° TO 360°. TO DETERMINE MINIMUM ANGLE BETWEEN ADAPTERS SEE DRAWING NO. 7001-110-012.

FILE NAME: A:\4700\us47786\038\1\W\CADD\Sheets\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2026, TIME: 5:05 PM, USER: ah5754

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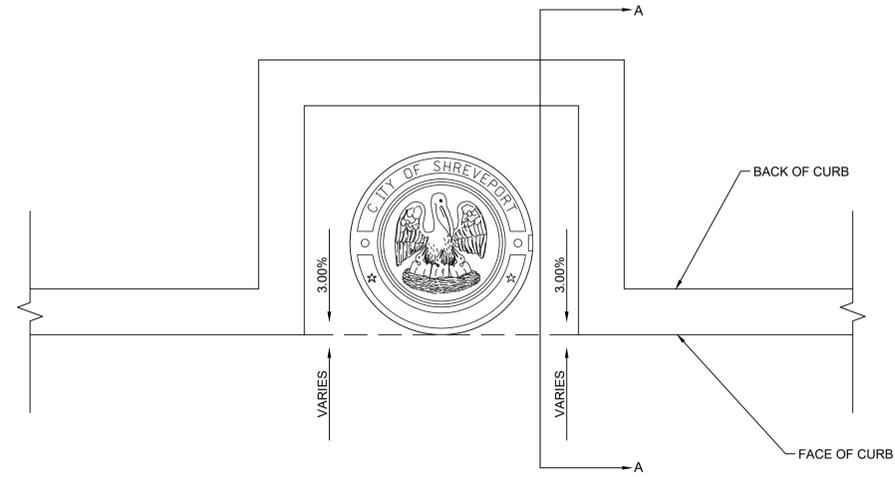
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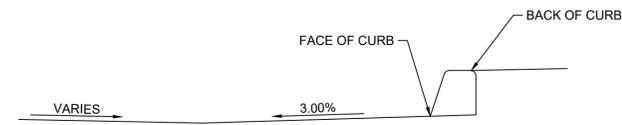
SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
 ISSUED: 2/17/26
 SCALE: N.T.S.
 SHEET TITLE
 STORM DRAIN
 DETAILS (5 OF 5)
 C10.7

FILE NAME: A:\4700\47786\038\DWG\CADD\Sheets\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2026, TIME: 5:05 PM, USER: ah5754



BOXOUT DETAIL
SCALE: N.T.S.



A-A
SCALE: N.T.S.

REVISION		
NO.	DATE	DESCRIPTION

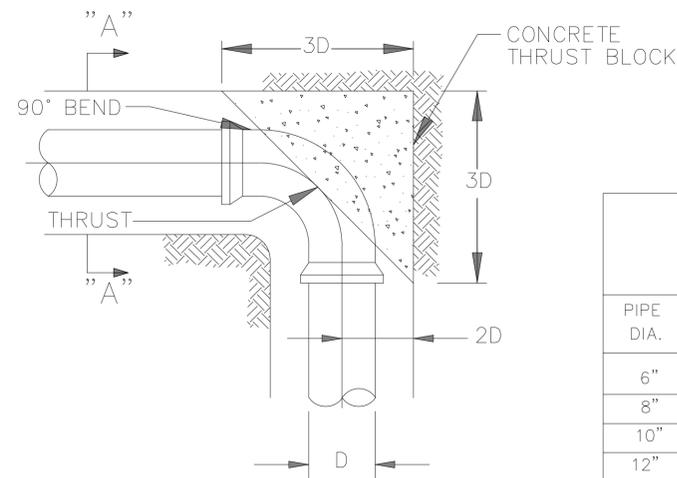
DESIGNED BY: JB
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SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/26
SCALE: N.T.S.
SHEET TITLE
BOXOUT DETAIL
C10.8

FILE NAME: A:\4700us\47786\038\DWG\CADD\Sheets\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2026, TIME: 5:05 PM, USER: ah5754

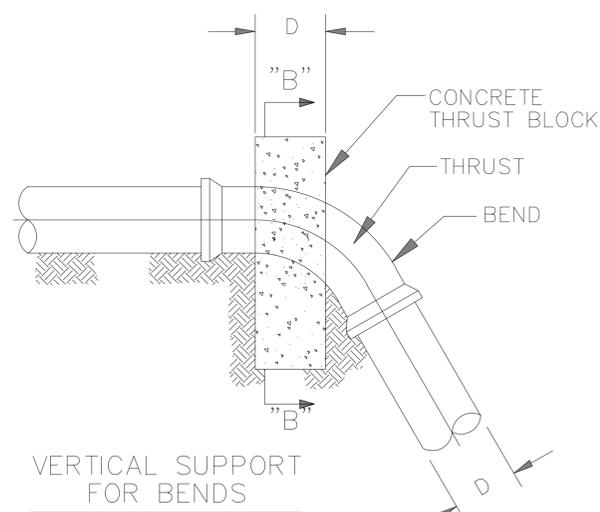


TYPICAL BLOCKING FOR BENDS 45° - 90°

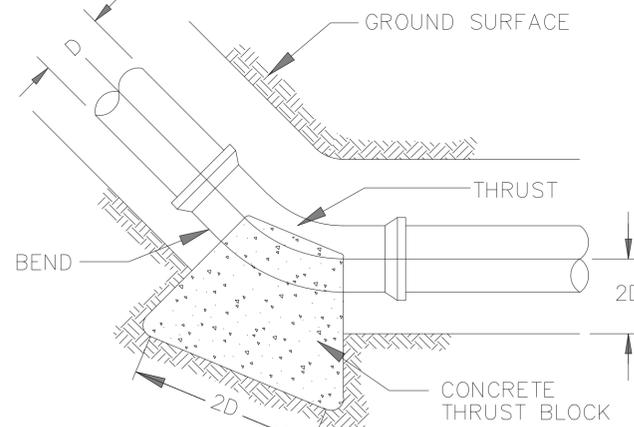
SECTION "A" SEE STANDARD PLAN 3000-6

PIPE DIA.	BEND "DEGREES"			
	90	45	22 1/2	11 1/4
6"	3	2	1	1
8"	5	3	1	1
10"	8	5	2	1
12"	12	6	3	2
16"	21	12	6	3
18"	27	15	7	4
20"	33	18	9	5
24"	48	26	13	7
30"	75	41	21	10
36"	108	58	30	15

1. SOIL BEARING CAPACITY BASED ON 3000 LB./SF. SURFACE AREA OF BEARING SOIL IS PROVIDED FOR 150 PSI MAXIMUM PRESSURE (INCLUDING SURGE), SAFETY FACTOR OF 1.5. AND 3000 LB./SF. SOIL BEARING PRESSURE. IF PRESSURE IS HIGHER OR SOIL BEARING IS POTENTIALLY LOWER, CONSULT THE ENGINEER FOR ADJUSTMENTS.
2. MAXIMUM PRESSURE 150 PSI. (INCL SURGE) AND A SAFETY FACTOR OF 1.5.
3. FOR OTHER SOIL BEARING PRESSURE AND PIPE PRESSURE CONDITIONS, CONSULT THE ENGINEER OF RECORD.



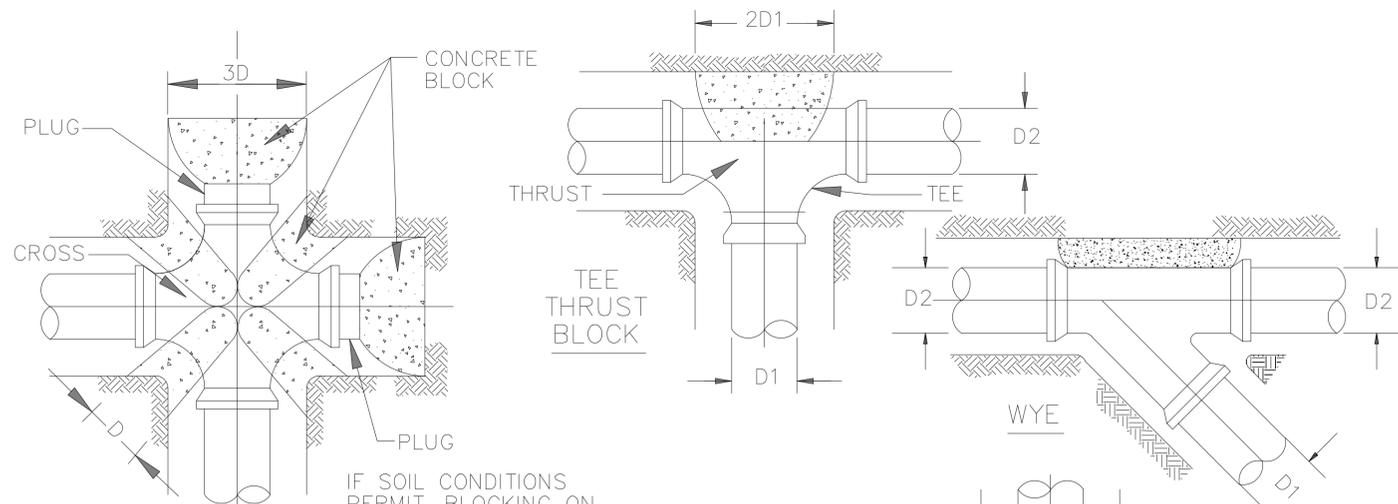
SECTION "B" SEE STANDARD PLAN 3000-6



TYPICAL BLOCKING FOR BENDS < 45°

NOTES:

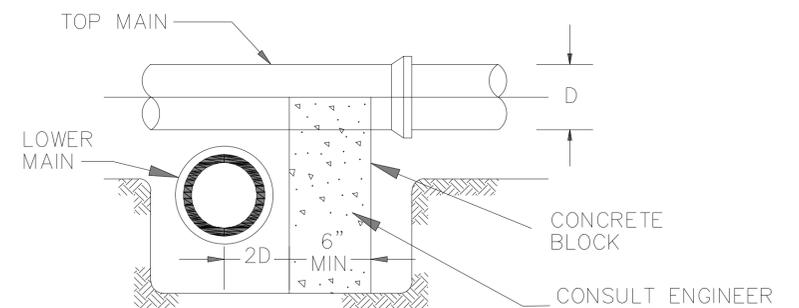
1. FOR PIPES 6" IN DIAMETER AND SMALLER THE "D" DIMENSION SHALL NOT BE LESS THAN 6".
2. CONCRETE THRUST BLOCK SHALL NOT LIMIT FLEXIBILITY OF JOINTS AND OTHER FITTINGS.
3. CONCRETE THRUST BLOCK SHALL BE CLASS "A" CONCRETE PER SPEC. 201.
4. PLACE BOND BREAKER BETWEEN PIPE AND CONCRETE



TYPICAL THRUST BLOCK CROSS AND TWO PLUGS

IF SOIL CONDITIONS PERMIT, BLOCKING ON PLUGS ONLY AT CROSSING WILL BE SUFFICIENT.

WHEN CROSSING NEAR BELL OF TOP MAIN, USE SUPPORT AS SHOWN. FOR CROSSINGS NEAR CENTER OF SPAN, USE SUPPORT ON EACH SIDE OF PIPE.

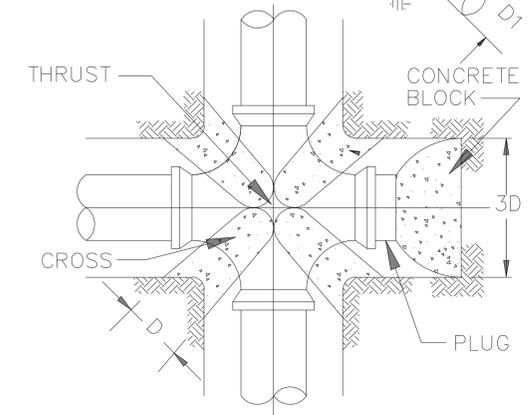


PIPE SUPPORT AT CROSSING OF MAINS

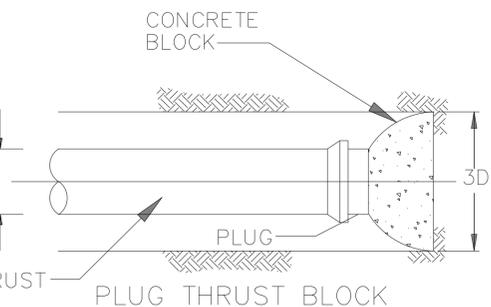
REQUIRED BEARING AREA ON UNDISTURBED SOIL AND TYPICAL DIMENSIONS	
TEES WYES & PLUGS	
SIZE	AREA SQ. FT.
6"	2
8"	4
10"	6
12"	8
14"	12
16"	15
18"	19
20"	24
24"	34
36"	76

NOTES:

1. THRUST BLOCKING SHALL NOT LIMIT FLEXIBILITY OF JOINTS AND OTHER FITTINGS.
2. THRUST BLOCKS FOR TEES SHALL EXTEND TO THE FULL LENGTH OF THE TEE.
3. BLOCKING SHALL BE CLASS "A" CONCRETE, PER SPECIFICATION SECTION 201.
4. SURFACE AREA OF BEARING SOIL IS PROVIDED FOR 150 PSI MAXIMUM PRESSURE (INCLUDING SURGE), SAFETY FACTOR OF 1.5. AND 3000 LB./SF. SOIL BEARING PRESSURE. IF PRESSURE IS HIGHER OR SOIL BEARING IS POTENTIALLY LOWER, CONSULT THE ENGINEER FOR ADJUSTMENTS.



TYPICAL THRUST BLOCK OF CROSS AND PLUG



PLUG THRUST BLOCK

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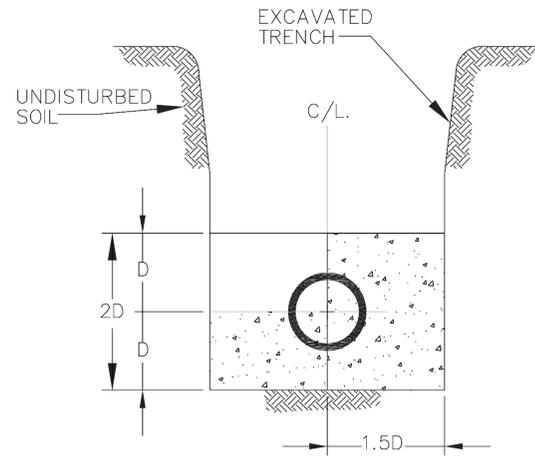
2/17/2026



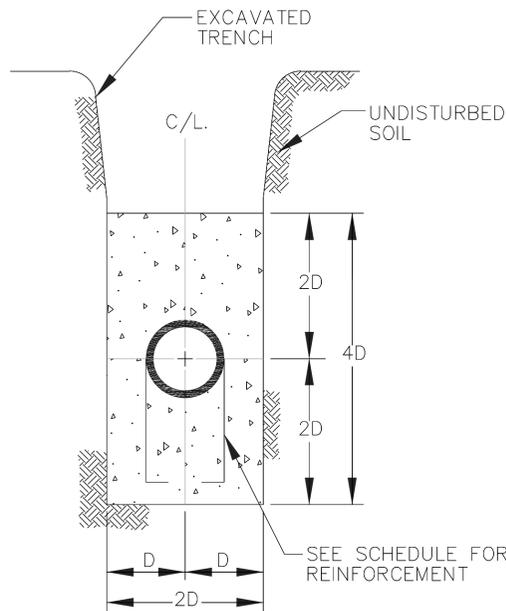
SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
 ISSUED: 2/17/26
 SCALE: N.T.S.
 SHEET TITLE: WATER DETAILS (1 OF 2)
 C10.9

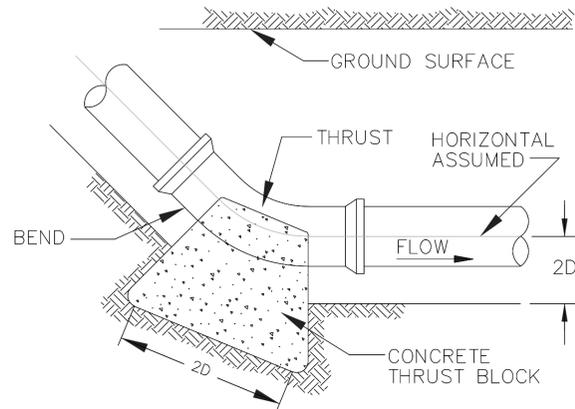
FILE NAME: A:\47000\47786\038\DWG\CADD\Sheets\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2026, TIME: 5:05 PM, USER: ah5754



SECTION "A - A"
(SEE STANDARD PLAN SHEET 3000-4)



SECTION "B - B"
(SEE STANDARD PLAN SHEET 3000-4)



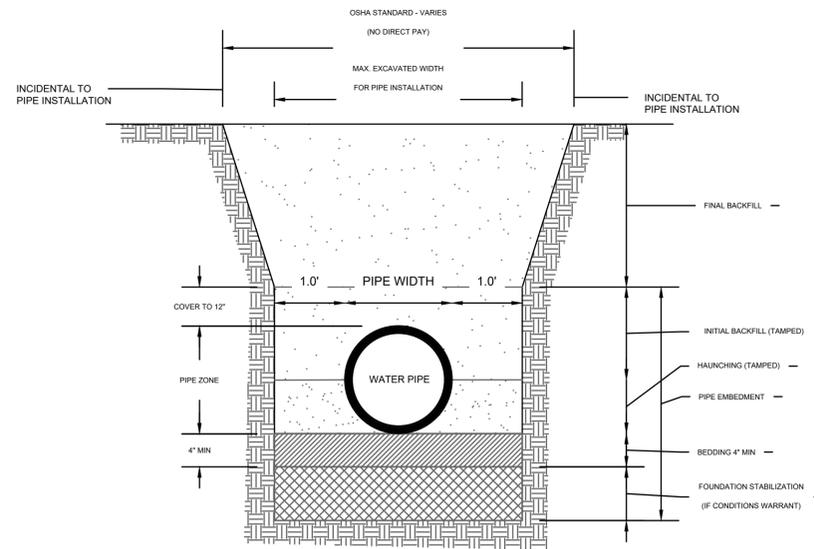
UNDER BREAK FOR VERTICAL PIPING

NOTES:

- THRUST BLOCKING SHALL NOT LIMIT FLEXIBILITY OF JOINTS AND OTHER FITTINGS.
- THRUST BLOCKS FOR TEES SHALL EXTEND TO THE FULL LENGTH OF THE TEE.
- BLOCKING SHALL BE CLASS "A" CONCRETE, PER SPECIFICATION SECTION 201.
- SURFACE AREA OF BEARING SOIL IS PROVIDED FOR 150 PSI MAXIMUM PRESSURE (INCLUDING SURGE), SAFETY FACTOR OF 1.5, AND 3000 LB./SF. SOIL BEARING PRESSURE. IF PRESSURE IS HIGHER OR SOIL BEARING IS POTENTIALLY LOWER, CONSULT THE ENGINEER FOR ADJUSTMENTS.

REINFORCEMENT SCHEDULE

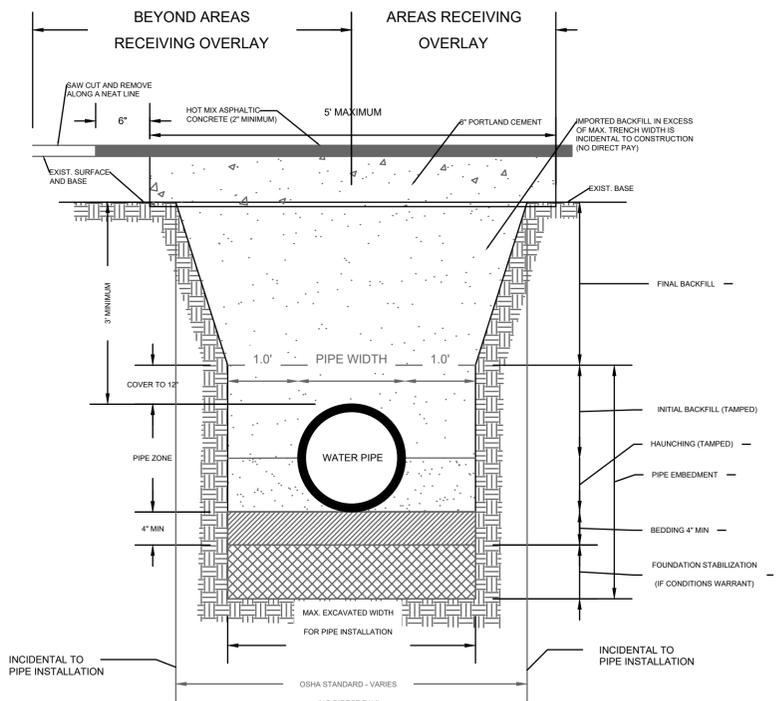
PIPE DIAMETER	VERTICAL THRUST UP (11 1/4' TO 90°)				HOOKS (EA. END)
	11 1/4' TO 22 1/2'	45' TO 90'			
10" OR LESS	1	NO. BARS	NO. BARS	NO. BARS	4"
12" TO 18"	2	SIZE BARS	SIZE BARS	SIZE BARS	6"
20" TO 36"	2	SIZE BARS	SIZE BARS	SIZE BARS	8"



WATER MAIN PIPE EMBEDMENT / BACKFILL

NOTES:
REFER TO SECTION 1010 FOR SPECIFICS
IMPORTED BACKFILL SHALL BE PAID AT THE CONTRACT UNIT BID RATE APPLICABLE AND PAID VOLUME IS LIMITED TO THE MAXIMUM TRENCH WIDTH.

- LEGEND:
- TAMPED BACKFILL IS DEFINED AS NATIVE SOIL, FREE OF ROCK, FOREIGN MATERIAL AND FROZEN EARTH.
 - SELECT MATERIAL BACKFILL IS DEFINED AS LOOSE NON-COHESIVE FINE-GRAINED SOIL.
 - COARSE AGGREGATE BACKFILL IS DEFINED AS MANUFACTURED ANGULAR AGGREGATE MATERIALS SUCH AS CRUSHED STONE OR ROCK.



WATER MAIN PIPE EMBEDMENT / BACKFILL AND PAVEMENT REPLACEMENT

- LEGEND:
- TAMPED BACKFILL IS DEFINED AS NATIVE SOIL, FREE OF ROCK, FOREIGN MATERIAL AND FROZEN EARTH.
 - SELECT MATERIAL BACKFILL IS DEFINED AS LOOSE NON-COHESIVE FINE-GRAINED SOIL.
 - COARSE AGGREGATE BACKFILL IS DEFINED AS MANUFACTURED ANGULAR AGGREGATE MATERIALS SUCH AS CRUSHED STONE OR ROCK.

NOTES:
NATIVE BACKFILL (NO DIRECT PAY), OR IF AUTHORIZED IN WRITING BY ENGINEER, IMPORTED BACKFILL SHALL BE PAID AT THE CONTRACT UNIT BID RATE, AS APPLICABLE, AND PAID VOLUME IS LIMITED TO THE MAXIMUM TRENCH WIDTH.

NO.	DATE	DESCRIPTION

DESIGNED BY: JB
DRAWN BY: JG
CHECKED BY: JB



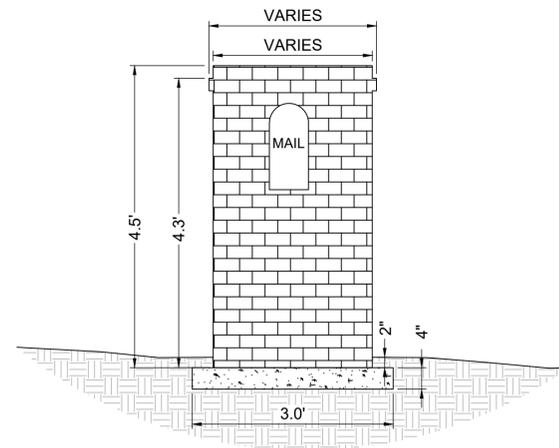
2/17/2026



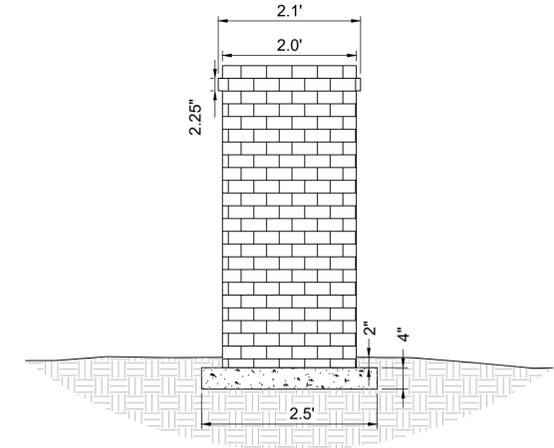
SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/26
SCALE: N.T.S.
SHEET TITLE
WATER DETAILS
(2 OF 2)
C10.10

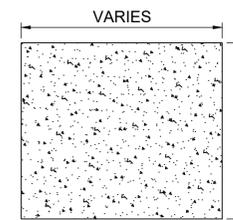
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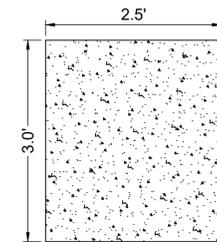
ELEVATION VIEW (FRONT)



ELEVATION VIEW (SIDE)



PLAN VIEW (FOUNDATION)



PLAN VIEW (FOUNDATION)

SINGLE MAILBOX STRUCTURE (BRICK)

NOT TO SCALE

TYPICAL MAILBOX SIZE IN INCHES			
SIZE	LENGTH	WIDTH	HEIGHT
SMALL	19 1/2	6	7
MEDIUM	22 1/2	8	11 1/2
LARGE	23 1/2 *	11 1/2 *	13 1/2 *

* Maximum allowed dimensions for mailbox

MAILBOX SIZES

NOTES:

1. ALL MAIL BOXES SHALL CONFORM TO U.S.P.S. STANDARDS.
2. ALL MAILBOXES TO BE REPLACED IN-KIND. BRICK COLOR AND STYLE TO MATCH EXISTING.
3. THE FRONT OF THE MAILBOX ITSELF SHALL NOT BE CLOSER THAN SIX (6) INCHES NOR FURTHER THAN FIFTEEN (15) INCHES FROM THE REAR EDGE OF THE CURB.
4. THE FIRST TWO TIERS OF MASONRY SHALL BE TIED INTO THE FOOTING AND NO LESS THAN 5/8-INCH RE-BAR IMBEDDED INTO THE OUTER FOOTING CONCRETE.
5. THE STRUCTURE SHALL NOT BE CONSTRUCTED SO AS TO PITCH MORE THAN 6 INCHES FROM THE CENTER LINE PERPENDICULAR TO THE REAR OF THE FOOTING NOR SHALL THE PORTION OF THE STRUCTURE THAT IS TO CONTAIN THE MAILBOX BE CANTILEVERED MORE THAN 6 INCHES FROM THE PERPENDICULAR PLANE OF THE STRUCTURE
6. FOR THE BRICK MAILBOXES FOUNDATION, THE CONCRETE STRENGTH SHALL BE AT MINIMUM CLASS A, 3,500 PSI CONCRETE.
7. SEE PART 7 OF THE PROJECT MANUAL FOR MASONRY CONSTRUCTION AND SPECIFICATION 702 FOR BRICK MASONRY.

REVISION		
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 CHECKED BY: JB

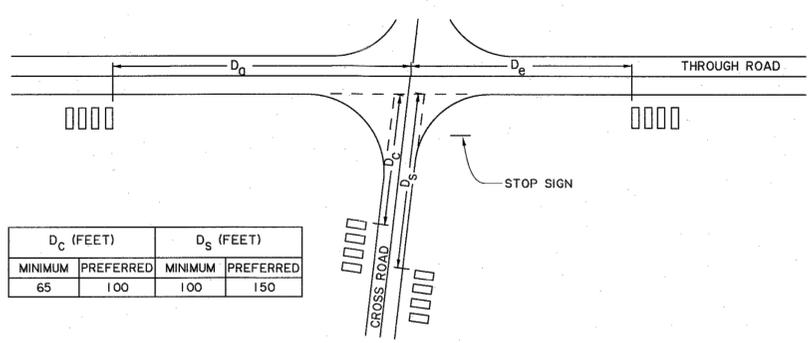


2/17/2026



SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

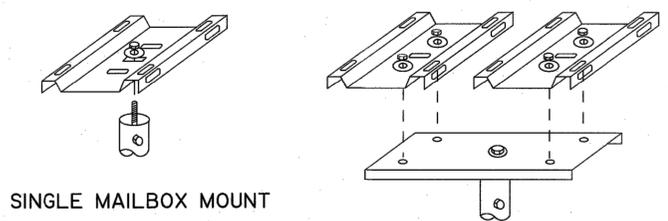
PROJECT NO.: 47786.038
 ISSUED: 2/17/26
 SCALE: N.T.S.
 SHEET TITLE
 MAILBOX DETAILS
 (1 OF 2)
 C10.11



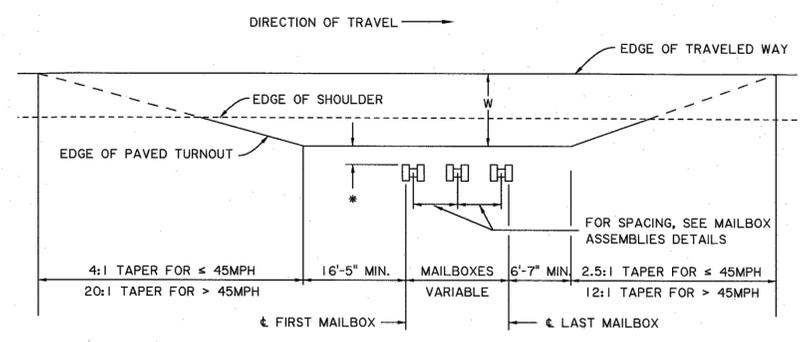
THROUGH ROAD SPEED (M.P.H.)	D _a (FEET)		D _e (FEET)	
	n V _c V _m ≤ 4000	n V _c V _m > 4000	$\frac{V_c}{1.5n-5} \leq 50$	$50 < \frac{V_c}{1.5n-5} \leq 400$
35	65	200	65	100
≥ 55	65	295	150	200

V_c = AVERAGE DAILY TRAFFIC ON CROSS ROAD (VEHICLES PER DAY)
 V_m = AVERAGE DAILY TRAFFIC ON THROUGH ROAD (VEHICLES PER DAY)
 n = NUMBER OF MAILBOXES AT MAIL STOP

MINIMUM CLEARANCES TO NEAREST MAILBOX IN MAIL STOPS AT INTERSECTIONS



SINGLE AND DOUBLE MAILBOX MOUNTS SERIES C



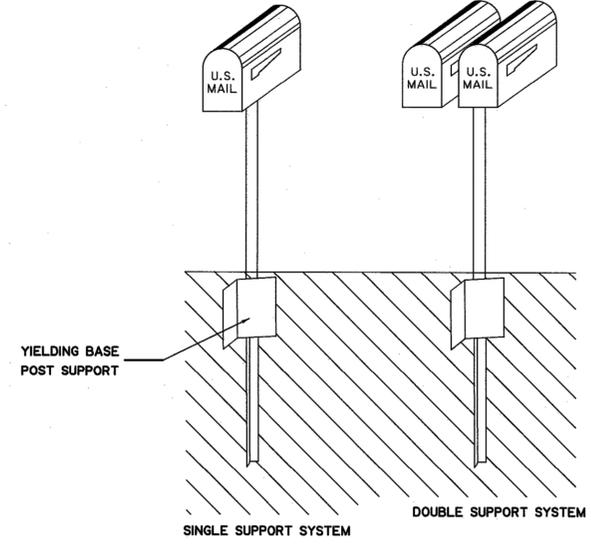
W = FOR SUGGESTED WIDTHS, SEE TABLE BELOW.
 MAILBOXES = A MINIMUM DESIGN FOR ROADS CARRYING LOW-SPEED TRAFFIC AND FOR LOCAL AND COLLECTOR ROADS.
 * = FOR MAILBOX FACE OFFSET, SEE TABLE BELOW, 0" - 12".

DETAIL OF MAILBOX TURNOUT

*** SUGGESTED GUIDELINES FOR LATERAL PLACEMENT OF MAILBOXES**

HIGHWAY TYPE AND ADT (VPD)	WIDTH OF ALL-WEATHER SURFACE TURNOUT OR AVAILABLE SHOULDER AT MAILBOX ^a (FT)		DISTANCE ROADSIDE FACE OF MAILBOX IS TO BE OFFSET BEHIND EDGE OF TURNOUT OR USABLE SHOULDER (IN)	
	PREFERRED	MINIMUM	PREFERRED	MINIMUM
RURAL HIGHWAY OVER 10,000	12	8	6 to 8	0
RURAL HIGHWAY 1,500 TO 10,000	12	8		
RURAL HIGHWAY 400 TO 1,500	10	8		
RURAL ROAD UNDER 400	8	6 ^b	6 ^c	6 ^d
RESIDENTIAL STREET WITHOUT CURB OR ALL-WEATHER SHOULDER	6	0		
CURBED RESIDENTIAL STREET OR URBAN AND SUBURBAN AREAS	NOT APPLICABLE		8 to 12 ^d	6 ^d

NOTES: ADT = AVERAGE DAILY TRAFFIC
 VPD = VEHICLES PER DAY
 a) IF INCREASE ACCESS IS NEEDED, THE FOLLOWING MAY BE CONSIDERED IN CONJUNCTION WITH THE LOCAL POSTMASTER:
 - PROVIDE A LEVEL CLEAR SPACE 30 IN. BY 48 IN. CENTERED ON THE BOX FOR EITHER SIDE OR FORWARD APPROACH.
 - PROVIDE AN ACCESSIBLE PASSAGE TO AND FROM THE MAILBOX AND PROJECT INTO A CIRCULATION ROUTE—NO MORE THAN 4 IN. IF BETWEEN 28 IN. AND 80 IN.—SO THAT THE MAILBOX DOES NOT BECOME A PROTRUDING OBJECT FOR PEDESTRIANS WITH IMPAIRED VISION.
 b) PROVIDE AN ACCESSIBLE PASSAGE TO AND FROM THE MAILBOX. THE MAILBOX PROJECTION INTO A CIRCULATION ROUTE SHALL NOT BE MORE THAN 4 IN., SO THAT THE MAILBOX DOES NOT BECOME A PROTRUDING OBJECT FOR PEDESTRIANS WITH IMPAIRED VISION.
 c) IF A TURNOUT IS PROVIDED, THIS MAY BE REDUCED TO ZERO.
 d) BEHIND TRAFFIC-FACE OF CURB.



EXAMPLES OF SINGLE AND DOUBLE MAILBOX INSTALLATIONS SERIES C

* NOTE: SUPPORT FRAME AND FOUNDATION SHOWN ARE PROPRIETARY PRODUCTS.

NOTES:

NO MAILBOX WILL BE PERMITTED WHERE ACCESS IS OBTAINED FROM THE LANES OF A FREEWAY OR WHERE ACCESS IS OTHERWISE PROHIBITED BY LAW OR REGULATION.

MAILBOXES SHALL BE LOCATED ON THE RIGHT-HAND SIDE OF THE ROADWAY IN THE DIRECTION OF DELIVERY ROUTE EXCEPT ON ONE-WAY STREETS WHERE THEY MAY BE PLACED ON EITHER SIDE. THE BOTTOM OF THE BOX SHALL BE SET AT AN ELEVATION (H) ESTABLISHED BY THE U.S. POSTAL SERVICE, USUALLY BETWEEN 3'4" AND 4'0" ABOVE THE ROADWAY SURFACE. THE ROADSIDE FACE OF THE BOX SHALL BE OFFSET FROM THE EDGE OF THE TRAVELED WAY. SEE THE SUGGESTED GUIDELINES FOR LATERAL PLACEMENT OF MAILBOXES AT LEFT.

ALL MAILBOX INSTALLATIONS MUST CONFORM TO THE REQUIREMENTS OF THE U.S. POSTAL SERVICE.

WHERE FEASIBLE, NEW INSTALLATION SHOULD BE LOCATED ON THE FAR RIGHT SIDE OF AN INTERSECTION WITH A ROAD OR DRIVEWAY ENTRANCE. HOWEVER, CONSIDERATION SHOULD BE GIVEN TO

- MINIMIZING WALKING DISTANCE WITHIN THE ROADWAY FOR THE PATRON,
- AVAILABLE STOPPING SIGHT DISTANCE IN ADVANCE OF THE MAILBOX SITE, AND
- POSSIBLE RESTRICTIONS TO CORNER SIGHT DISTANCES AT INTERSECTIONS AND DRIVEWAY ENTRANCES.

FOR LOCATION OF MAILBOXES AT AN INTERSECTING ROADWAY, SEE DETAIL AT LEFT.

MAILBOXES SHALL BE OF LIGHT SHEET METAL OR PLASTIC CONSTRUCTION MANUFACTURED BY AN APPROVED MANUFACTURERS CONFORMING TO THE REQUIREMENTS OF THE U.S. POSTAL SERVICE. MAILBOXES MUST BE FULL-SCALE CRASH TESTED IN ACCORDANCE WITH THE LATEST EDITION OF MASH. NEWSPAPER DELIVERY BOXES SHALL BE OF LIGHT SHEET METAL OR PLASTIC CONSTRUCTION OF MINIMUM DIMENSIONS SUITABLE FOR HOLDING A NEWSPAPER.

MANUFACTURERS WHOSE MAILBOXES HAVE BEEN APPROVED BY THE POSTMASTER GENERAL WILL BE LISTED IN THE POSTAL OPERATION MANUAL (POM) AND PUBLISHED IN THE POSTAL BULLETIN. NO MORE THAN TWO MAILBOXES MAY BE MOUNTED ON A SUPPORT STRUCTURE UNLESS THE SUPPORT STRUCTURE AND MAILBOX ARRANGEMENT HAVE BEEN SHOWN TO BE SAFE BY CRASH TESTING IN ACCORDANCE WITH THE LATEST EDITION OF MASH. HOWEVER, LIGHTWEIGHT NEWSPAPER BOXES MAY BE MOUNTED BELOW THE MAILBOX ON THE SIDE OF THE MAILBOX SUPPORT.

MAILBOX SUPPORTS SHALL NOT BE SET IN CONCRETE UNLESS THE SUPPORT DESIGN HAS BEEN SHOWN TO BE SAFE BY CRASH TESTING IN ACCORDANCE WITH THE LATEST EDITION OF MASH WHEN SO INSTALLED.

POSTS SHALL BE STRONG ENOUGH TO SUPPORT THE BOX, BUT CAPABLE OF BENDING WHEN STRUCK BY AN AUTOMOBILE OR A LIGHT TRUCK. MAXIMUM STRENGTH POSTS ARE EITHER A METAL POST WITH A STRENGTH NO GREATER THAN A 2" DIAMETER STANDARD STRENGTH STEEL PIPE OR A 2"x4" FLANGED CHANNEL OR A 4"x4" WOODEN POST. POSTS ARE ACCEPTABLE MAILBOX SUPPORTS WHEN EMBEDDED NO MORE THAN 24" INTO THE GROUND. A METAL POST SHALL NOT BE FITTED WITH AN ANCHOR PLATE, BUT MAY HAVE AN ANTI-TWIST DEVICE THAT EXTENDS NO MORE THAN 10" BELOW THE GROUND SURFACE.

THE POST-TO-BOX ATTACHMENT SHALL BE OF SUFFICIENT STRENGTH TO PREVENT THE BOX FROM SEPARATING FROM THE POST TOP IF THE INSTALLATION IS STRUCK BY AN AUTOMOBILE OR LIGHT TRUCK.

THE MINIMUM SPACING BETWEEN THE CENTERS OF SUPPORT POSTS SHALL BE THREE-FOURTHS THE HEIGHT OF THE POSTS ABOVE THE GROUND LINE

MAILBOX SUPPORT DESIGNS NOT DETAILED WILL BE ACCEPTABLE IF FULL-SCALE CRASH TESTED IN ACCORDANCE WITH THE LATEST EDITION OF MASH AND IF APPROVED BY THE ENGINEER.

FOR POST-TO-BOX ATTACHMENT DETAILS, SEE SHEET 2 OF 2.

MASH - MANUAL FOR ASSESSING SAFETY HARDWARE.

SHEET NUMBER	
DESIGN	H. VU
CHECK	D. SMITH
DETAIL	H. VU
CHECK	D. SMITH
REVIEW	
SERIES	1 OF 2

STATE OF LOUISIANA
 HAI D. VU
 REG. NO. 34755
 REGISTERED PROFESSIONAL ENGINEER
 CIVIL ENGINEERING
 04/04/22

APPROVED BY CHIEF ENGINEER: *Christopher P. Hobbs*
 DATE: 4/4/2022

MAILBOX INSTALLATION DETAILS
 MB-01

DOTD
 LOUISIANA DEPARTMENT OF TRANSPORTATION & DEVELOPMENT
 ROAD DESIGN

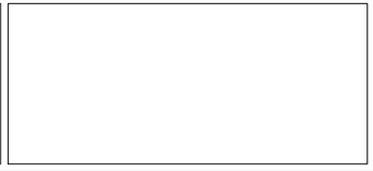
FILE NAME: A:\47000\47786\038\1\PCADD\Sheet\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2026, TIME: 5:05 PM, USER: ah5754

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 DRAWN BY: JG
 CHECKED BY: JB

halff
 401 MARKET ST.
 SUITE 650
 SHREVEPORT, LA 71101
 TEL. (318) 716-6136

STATE OF LOUISIANA
 JARED R. BOOGHEYS
 REG. NO. 41026
 REGISTERED PROFESSIONAL ENGINEER
 CIVIL ENGINEERING
 2/17/2026



SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
 ISSUED: 2/17/26
 SCALE: N.T.S.
 SHEET TITLE
 MAILBOX DETAILS (2 OF 2)
 C10.12

FILE NAME: A:\47000us\47786\038\PC\CADD\Sheets\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2026, TIME: 5:06 PM, USER: ah5754

GENERAL PROVISIONS

- All temporary traffic control (TTC) devices used shall be in accordance with the Louisiana Standard Specifications for Roads and Bridges, the MUTCD, and shall meet the NCHRP Report 350 or MASH requirements for Test Level 3 devices where applicable.
Materials used for TTC shall be in accordance with the Louisiana Standard Specifications for Roads and Bridges and, when applicable, the LADOTD AML.
Placement of TTC devices shall not commence without the approval of the Engineer and until work is about to begin, unless they are covered.
No lane closures, lane shifts, diversions or detours shall occur without the approval of the Engineer.
Responsibility is hereby placed upon the contractor for the installation, maintenance and operation of all TTC devices called for in these plans or required by the Engineer for the protection of the traveling public as well as all LADOTD and construction personnel.
The contractor shall also be responsible for the maintenance of all permanent signs, pavement markings, and traffic signals left in place as essential to the safe movement and guidance of traffic within the project limits unless noted in the plans.
The DTOE shall serve as a technical advisor to the Engineer for all traffic control matters.
The Chief Construction Engineer or his appointed designee shall approve all signs and situations not addressed in the plans based on the recommendations of the Project Engineer and the DTOE. All changes shall be noted in all project traffic control diaries.
The Chief Construction Engineer or his appointed designee shall approve all design speeds of diversions or shifts, if it differs from design plans, based on the recommendations of the Project Engineer and the DTOE.
All temporary traffic control plans shall comply with the Transportation Management Plan.
Any additional signs shown in the MUTCD and required by the Engineer shall be installed under Item 713-01-00100.
Neither work activity nor storage of equipment, vehicles, TMAs, or materials shall occur within the buffer space.
When a work area has been established on one side of the roadway only, there shall be no conflicting operations or parking on the opposite shoulder within 500 feet of the work area.
A lighting plan shall be submitted to the Engineer 30 days prior to night work for approval. (See section 713.10 of the Louisiana Standard Specifications for Roads and Bridges.)
Parking of vehicles or unattended equipment or storage of materials, within the work zone clear zone shall not be permitted unless protected by guardrail or barriers. If the work zone clear zone is not defined on the plan sheets, the Engineer shall verify.
Immediately upon removal of existing guardrail, the contractor shall install and maintain an NCHRP Report 350 or MASH approved device to protect the blunt end of the bridge or column until new guardrail is installed. After removal of the existing guardrail, new guardrail should be installed within seven (7) days. On non-NHS routes with shoulders less than 8 feet wide: If an NCHRP 350 Report Test Level 3 or MASH device is required but the field conditions of the roadway cannot support a Test Level 3 device, then a Test Level 2 device can be substituted in its place upon approval by the Engineer. If utilized, a TMA is allowed for a maximum of 72 hours.
All costs associated with temporary crash devices are to be included under the appropriate NS-700 pay item.
Sight distance should be considered when placing traffic control devices.
On all mainline Interstates, a minimum of 1.5 feet of paved shoulder on the left and right side shall be maintained at all times.

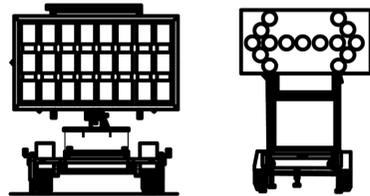
- On Interstates, a minimum of 11 foot lanes shall be maintained. On all other roadways, a 10 foot minimum travel lane should be maintained where practical.
TTC Standards are not drawn to scale.
The contractor shall develop an internal traffic control plan approved by the Engineer prior to each phase.
Truck restrictions such as (but not limited to) restricting lanes, oversize loads or times of travel, may be required for narrow lanes or other field conditions.
Temporary concrete barrier shall be placed on a paved surface. This paved surface should follow current design criteria used for paved embankment widening for guardrails.
Flare rates for temporary concrete barriers should follow the most current guidance in the AASHTO Roadside Design Guide.

PAVEMENT MARKINGS (see AML)

- All pavement markings within the limits of the project or adjacent to the project limits that are in conflict with the project signing or the required traffic movements shall be removed from the pavement by blast cleaning or grinding. (Existing striping shall not be painted over with black paint or covered with tape.)
If special pavement markings are needed, they shall be reflectorized, removable and accompanied by the proper signage.
Temporary Raised Pavement Markers may be added to supplement temporary striping in areas of transition, in tapers, in diversions and in other areas of need as shown in the plans or as directed by the Engineer.
Temporary markings installed in the permanent configuration shall comply with LADOTD pavement marking standard plans, MUTCD and/or the permanent striping plans.

PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS)

- PCMS shall be used on all Interstate Highways. PCMS shall be used on all other roadways (where space is available) with an ADT greater than 20,000.
When used in advance of a lane closure or a lane shift, the PCMS should be placed on the right hand side of the road a minimum distance of 2 miles in advance of the taper for interstates and to be determined by the Engineer on other highways.
For interstates and multi-lane highways, if vehicles are queuing beyond the 2 mile PCMS, an additional PCMS should be placed on the right hand side of the road approximately 5 miles in advance of the taper or at the end of the queue, whichever is greater.
PCMS messages shall be approved by the Engineer. Messages shall be no more than 3 lines and 2 screens.
Messages shall display only traffic operational, regulatory, warning, and guidance information. PCMS messages shall not display advertising or safety messages. Messages should only convey information concerning the problem/situation, location, and recommended driver action.
PCMS should be placed as far from the traveled lane as possible. They shall be shielded by guardrail or barriers. If this is not possible they shall be delineated with a min. 3 drum taper spaced at 20ft with a 4th drum alongside the PCMS.
If the PCMS encroaches on the improved shoulder then the contractor shall install a shoulder closure.
When the PCMS is not displaying a work zone appropriate message pertaining to the ongoing construction project it shall be shielded by guard rail or barriers, or removed from the work zone clear zone.



ALL TTC STANDARDS SHOW MINIMUM CONSTRUCTION SIGNING. ALL SITUATIONS SHALL BE REVIEWED AND/OR DESIGNED BY THE ENGINEER. CONTRACTORS ARE RESPONSIBLE FOR COMPLYING WITH ALL TTC STANDARDS.

SPEED LIMITS

- The Engineer may approve a 10 mph drop in the speed limit for posted speeds of 45 mph or greater and for any construction, maintenance or utility operation that requires one or more of the following:
(A) The condition of the traveled way is degraded due to milled surfaces or uneven travel lane lines greater than 1.5 inches.
(B) Work is in progress in the immediate vicinity of the travel way requiring lane closures or lane width reductions less than 11 feet.
(C) Workers present on the shoulder within 2 feet of the edge of the traveled way without barrier protection.
The reduced speed zone shall only apply to those portions of the project limits affected. The Engineer may allow SPEED LIMIT WHEN FLASHING signs to supplement reduced speed zones.
If the speed limit is reduced, speed limit signs shall be placed:
(A) beyond major intersections;
(B) at one mile intervals in rural areas;
(C) at half mile intervals in urban areas.
At the end of the reduced speed zone, a speed limit sign displaying the original speed limit prior to construction shall be installed.
For all other speed limit reductions not listed above, the Project Engineer and the DTOE shall recommend the speed reduction to the Chief Construction Engineer or his appointed designee for approval.
If the speed limit is reduced more than 10 mph, placement of the signs shall be re-evaluated according to the MUTCD.

FLASHING ARROW BOARDS

- All Flashing Arrow Boards shall be 4 feet by 8 feet and Type C.
Flashing Arrow Boards should be placed on the shoulder. When there is no shoulder or median area, the arrow board shall be placed within the closed lane behind the channelizing devices and as close to the beginning of the taper as practical.
Flashing arrow boards shall be delineated with retroreflective TTC devices.
At no time shall the arrow board encroach in the traveled way. When Flashing Arrow Board signs are not being used, they shall be shielded by guard rail or barriers, or removed.
Arrow boards shall only be used for lane reduction tapers and shall not be used for lane shifts.

ABBREVIATIONS

- AASHTO American Association of State Highway and Transportation Officials
ADT Average Daily Traffic
AGC Associated General Contractors of America
AML Approved Materials List
ANSI American National Standards Institute
ATSSA American Traffic Safety Services Association
B.O.P. Beginning of Project
DTOE District Traffic Operations Engineer
E.O.P. End of Project
LADOTD Louisiana Department of Transportation and Development
MASH AASHTO Manual for Assessing Safety Hardware
MUTCD Manual on Uniform Traffic Control Devices
NCHRP National Cooperative Highway Research Program
NHS National Highway System
PCMS Portable Changeable Message Sign
TMA Truck Mounted Attenuator
TMC Traffic Management Center
TTC Temporary Traffic Control
TTC Standards .. Temporary Traffic Control Standard Plans

Vertical sidebar containing project information, approval signatures, and logos for DOTD and the City of Shreveport.

Table with 3 columns: REVISION NO., DATE, DESCRIPTION. Includes a warning triangle icon.

JB DESIGNED BY:
JG DRAWN BY:
JB CHECKED BY:

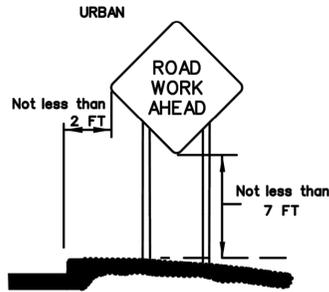


SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/26
SCALE: N.T.S.
SHEET TITLE: LADOTD STANDARD DETAILS (1 OF 6)
C10.13

SIGNS

- All signs used for temporary traffic control shall follow the plans, the LADOTD TTC Standards and the MUTCD.
- Signs shown in the TTC illustrations are typical and may vary with each specific condition.
- One Type B High Intensity light shall be used to supplement the first sign (or pair of signs) that gives warning about a lane closure during nighttime operations (See AML).
- Mesh rollup signs shall not be allowed on any project.
- Contractor shall use caution not to damage existing signs which remain in place. Any LADOTD signs damaged by work operations shall be replaced by the contractor under item 713-01-00100.
- All signs (permanent and temporary) shall be removed or completely covered with a strong, lightweight, opaque material when no longer applicable. (Burlap is not an acceptable material to cover signs).
- At no time shall signs warning against a particular operation be left in place once the operation has been completed or where the condition has been removed.
- Warning signs used for temporary traffic controls shall meet the following guidelines unless otherwise noted in the plans:
 - (A) size shall be 48 inches by 48 inches.
 - (B) see the Louisiana Standard Specifications for Roads and Bridges and the AML for sheeting information.
 - (C) lateral distance of signs shall be a minimum of 6 feet from the edge of shoulder or edge of pavement if no shoulder exists and 2 feet from the back of curb in urban areas (see diagram).
- When portable sign frames are not in use, they shall be moved to an area inaccessible to traffic and not visible to the driver.
- Left side mounted signs will not be required for roadways with a center left turn lane and for undivided roadways.
- Vinyl roll up signs and 1 foot portable sign stands may be used if work zone is in place for 3 days or less. Signs or stands may not be used if there are more than 2 lanes in each direction and if signs do not meet all size, color, retroreflectivity and NCHRP 350 Report or MASH requirements.
- All signs shall be visible to the drivers (i.e. no obstructions such as on street parking or other traffic control devices shall block the sign).
- On divided highways, signs shall be placed on the right and the left as shown on the TTC standards.
- Sign posts:
 - Signs measuring 10 square feet or less shall be mounted on 1 rigid post
 - Signs over 10 square feet shall be mounted on 2 rigid posts
 - Signs over 20 square feet shall be mounted on at least 3 rigid posts
- Rigid sign supports shall be driven to a minimum depth of 3 feet. (If splicing is required, see Allowable Lap Splice U-channel Post.)
- For sign height, see the Rural and Urban diagrams:

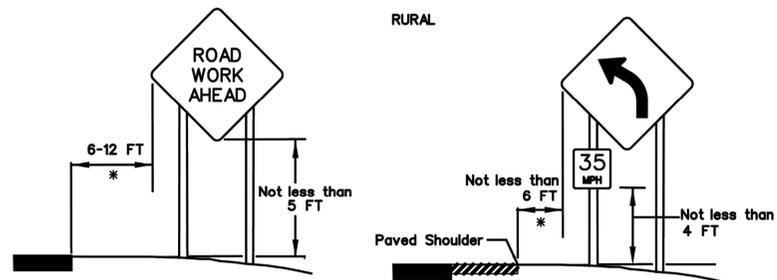


LANE CLOSURES

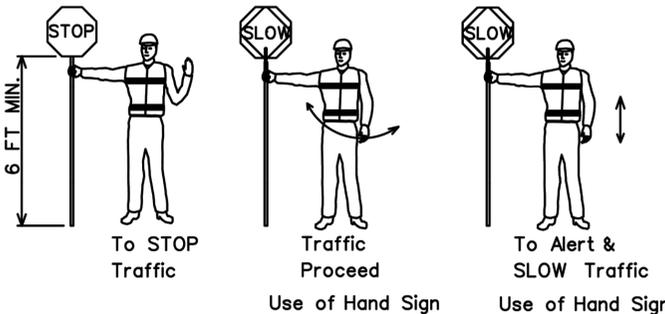
- All proposed lane, road or shoulder closures shall be reviewed by the DTOE and approved by the Engineer.
- Two lane, two-way highways shall have a maximum work area of two miles; all other roadways shall have a four mile maximum work area.
- A queue analysis shall be performed prior to approval of lane closures on all Interstates according to Section 6A.1 of the Traffic Engineering Manual.
- Closure plans and times shall be turned in to the Engineer for review according to the following:
 - (A) 5 working days minimum if traffic control plan has been approved or is contained in the plans.
 - (B) 10 working days minimum and a traffic control plan must be submitted for lane closures not addressed in the plans.
- Weekly updates to the DTOE, Project Engineer, the LADOTD TMC operator and the regional TMC operator (if applicable) will be required for all ongoing lane closures to update the closure status.
- Daily updates to the DTOE, Project Engineer and TMC operator (if applicable) will be required for all projects where active closures are in place.

FLAGGERS

- All flaggers shall be qualified.
- The contractor shall be responsible for training or assuring that all flaggers are qualified to perform flagging duties.
- A Qualified Flagger is one that has completed courses such as those offered by ATSSA or other courses approved by the LADOTD Work Zone Task Force. The contractor shall be responsible for getting the flagger course approved.
- When utilized, a flagger shall use a minimum 18 inch octagonal shape sign on a minimum 6 foot stop/slow paddle and wear ANSI Class 2 Lime Green vest during day time operations and ANSI Class 3 Lime Green ensemble during night operations.
- In all flagging operations, the flagger must be visible from the flagger advance warning sign.
- Flaggers shall not be used on the Interstate.



* If lateral distance is not practical, the sign may be placed no less than 2 feet.



PEDESTRIAN CONSIDERATIONS

- If the TTC zone affects the movement of pedestrians, adequate pedestrian access and walkways shall be provided either through the TTC zone or a designated alternate route.
- Pedestrians should be provided with a convenient and accessible path that replicates as nearly as practical the most desirable characteristics of the existing sidewalk(s) or footpath(s).
- Advance notification of sidewalk closures shall be provided by the maintaining agency.
- No storage of construction materials, equipment, and/or vehicles will be permitted on permanent or temporary bicycle, pedestrian, or transit facilities for any duration of time.

REFERENCES

- The contractor shall be responsible for understanding all rules and requirements in the current edition of the following documents:
 - 1) Louisiana Standard Specifications for Roads and Bridges. http://www.dotd.la.gov/Inside_LaDOTD/Divisions/Engineering/Standard_Specifications
 - 2) Manual on Uniform Traffic Control Devices for Streets and Highways (MUTCD). <http://mutcd.fhwa.dot.gov/>
 - 3) LADOTD Approved Materials List (AML) Manual. http://wwwsp.dotd.la.gov/Inside_LaDOTD/Divisions/Engineering/Materials_Lab/Pages/Menu_QPL.aspx
 - 4) LADOTD Traffic Engineering Manual http://wwwsp.dotd.la.gov/Inside_LaDOTD/Divisions/Engineering/Traffic_Engineering/Misc%20Documents/Traffic%20Engineering%20Manual.pdf
 - 5) National Cooperative Highway Research Program (NCHRP) Report 350: "Guidelines for Work Zones Traffic Control Devices". http://onlinepubs.trb.org/Onlinepubs/nchrp/nchrp_rpt_350-a.pdf
 - 6) NCHRP Report 475: "A Procedure for Assessing and Planning Nighttime Highway Construction and Maintenance". http://onlinepubs.trb.org/Onlinepubs/nchrp/nchrp_rpt_475.pdf
 - 7) NCHRP Report 476: "Guidelines for Design and Operation of Nighttime Traffic Control for Highway Maintenance". http://onlinepubs.trb.org/Onlinepubs/nchrp/nchrp_rpt_476.pdf
 - 8) NCHRP Report 498: "Illumination Guidelines for Nighttime Highway Work". http://onlinepubs.trb.org/Onlinepubs/nchrp/nchrp_rpt_498.pdf
 - 9) American Association of State Highway and Transportation Officials (AASHTO) Roadside Design Guide.
 - 10) American Traffic Safety Services Association (ATSSA) Quality Guidelines for Work Zone Traffic Control Devices and Features.
 - 11) U.S. Department of Transportation Federal Highway Administration Traffic Control Handbook for Mobile Operations at Night. <http://www.dot.state.il.us/blr/I023.pdf>

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SHEET NUMBER		PARISH		CONTROL SECTION		STATE PROJECT	
DESIGN		CHECK		DETAIL		CHECK	
B. BOUCHER		M. RILOVICH					
APPROVED BY CHIEF ENGINEER						DATE: Feb 7, 2025	
						BRB BY	
						STORAGE NEAR SIDEWALKS AND MINOR CORRECTIONS REVISION OR CHANGE ORDER DESCRIPTION	
						4-3-24 DATE	
						NO.	
						TTC-00 (B)	
						TEMPORARY TRAFFIC CONTROL GENERAL NOTES SHEET	
						DOTD	
						LOUISIANA DEPARTMENT OF TRANSPORTATION & DEVELOPMENT	
						TRAFFIC ENGINEERING	

FILE NAME: A:\47000us\47786\038\PW\CADD\Sheets\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2025, TIME: 5:06 PM, USER: ah5754

NO.	DATE	DESCRIPTION

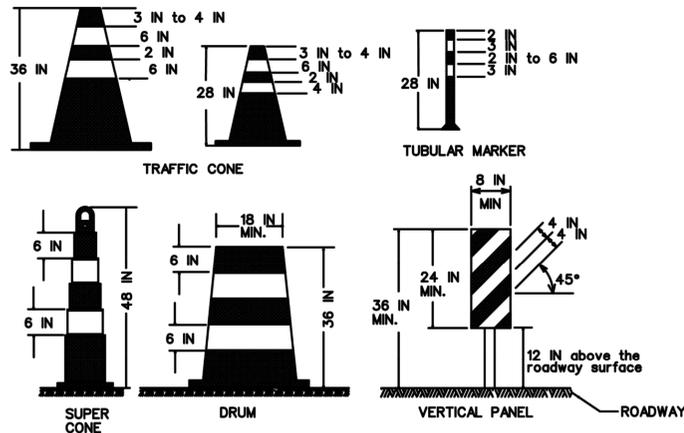
DESIGNED BY: JB
 DRAWN BY: JG
 CHECKED BY: JB

SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
 ISSUED: 2/17/26
 SCALE: N.T.S.
 SHEET TITLE
 LADOTD STANDARD
 DETAILS (2 OF 6)
 C10.14

CHANNELIZING DEVICES

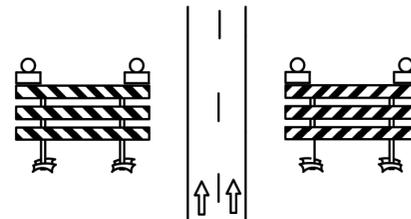
- The following devices may be used as channelizing devices: Tubular Markers, Vertical Panels, Cones, Drums and Super Cones.
- 28 inch traffic cones are not allowed on:
 - Interstates
 - Highways with speeds greater than 40 mph.
- During nighttime operations, 28 inch and 36 inch cones are not allowed.
- Retroreflective material pattern used on super cones shall match that used on drums.
- Tangent Areas:**
 - Standard Spacing:** See Standard Device Spacing and Buffer Space table.
 - Daylight Operations:** Drums and super cones are spaced at standard spacing. All other devices are at 1/2 standard spacing.
 - Nighttime Operations:** Drums and supercones at standard spacing are the only devices allowed.
- Taper Areas:**
 - Standard Spacing:** See Standard Device Spacing and Buffer Space table.
 - Daylight Operations:** Drums are spaced at standard spacing. All other devices are 1/2 standard spacing.
 - Nighttime Operations:** Drums (at standard spacing) are the only devices allowed.
- Downstream Locations & Flaggers:** Drums or supercones at 20' max spacing. The length of taper shall be between 50' - 100' with a minimum of 6 devices.
- Type C steady burn lights shall be used on all channelizing devices in the taper as well as the first two devices in the tangent at night, (see the AML).
- Typical channelizing device lateral placement (do not include when it is used as a divider for opposing directions of traffic) shall be 2 feet off the lane line in the closed lane or shoulder.
- Devices may be adjusted laterally to accommodate ongoing work in the immediate vicinity but must be returned to the closed lane after the work activity has moved.
- Channelizing devices on the lane line shall be of the same type.
- Channelizing devices in each taper shall be of the same type.



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TYPE III BARRICADES

- Only Type III Barricades shall be used in the roadway or shoulder. All barricades shall use Type 3 High Intensity Sheeting on both sides of the barricade.
- All barricades shall be a minimum of 8 feet in length and must meet NCHRP Report 350 or MASH requirements.
- When used for overnight closures, two Type B High Intensity Lights shall supplement all barricades that are placed in a closed lane or that extend across a highway. Two Type A Low Intensity Lights may be used in urban areas if approved by the Engineer (See AML).
- When signs and lights are to be mounted to a barricade, they must meet NCHRP Report 350 or MASH requirements.
- A truck with a TMA may be substituted for a barricade when workers are present.
- Barricades shall be placed:
 - at the beginning of a closed lane or shoulder and at 1,000 foot intervals where no active work is ongoing and the lane must remain closed. A minimum of 2 barricades shall be placed if the lane or shoulder closure is less than 2,000 feet. (One barricade shall be placed at the beginning of the lane closure after the buffer space and one shall be placed in the middle of the lane closure.)
 - before each or group of unfilled holes or holes filled with temporary material.
 - before uncured concrete.
 - in the closed lane on each side of every intersection and crossover. (Do not block sight distance.)
 - in front of piles of material (dirt, aggregate, broken concrete), culverts and equipment which is near the work zone.



TTC for DROP-OFFS

Average Drop-off	Current Posted Speed (Prior to Construction)	
	> 45 MPH	≤ 45 MPH
≤ 3 IN	Low Shoulder Sign (Optional)	Low Shoulder Sign (Optional)
> 3 IN	Shoulder Drop Off Sign & Edge Lines or Shoulder Drop Off Sign & Channelizing Device	Shoulder Drop Off Sign
> 6 IN	No Shoulder Sign, Edge Lines & Vertical Panel	No Shoulder Sign & Channelizing Device
> 10 IN	Concrete Barrier (if drop off is < 12 FT from edge of travel lane) & Edge Lines	No Shoulder Sign & Vertical Panel

Average Drop-off	Low Shoulder Sign (Optional)
≤ 2 IN	Low Shoulder Sign (Optional)
> 2 IN	Shoulder Drop Off Sign & Edge Lines or Shoulder Drop Off Sign & Channelizing Device
> 6 IN	Concrete Barrier (if drop off is < 12 FT from edge of travel lane), Shoulder Drop Off Sign, & Edge Lines

- If a portable concrete barrier will be required then the deflection shall be considered in the design.
- For Interstate ramps, refer to non-Interstate drop offs.

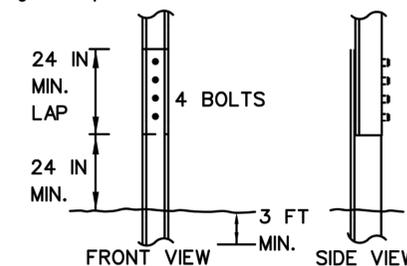
STANDARD DEVICE SPACING AND BUFFER SPACE

SPEED LIMIT (prior to construction)	MERGING TAPER LENGTH (L)				STANDARD DEVICE SPACING IN FEET		BUFFER SPACE
	Lane Width (FT)				Along Taper	Along Tangent	
MPH	9	10	11	12			FT
25	94	105	115	125	20	40	155
30	135	150	165	180	30	60	200
35	184	205	225	245	35	70	250
40	240	267	294	320	40	80	305
45	405	450	495	540	40	80	360
50	450	500	550	600	40	80	425
55	495	550	605	660	40	80	495
60	540	600	660	720	40	80	570
65	585	650	715	780	40	80	645
70	630	700	770	840	40	80	730
75	675	750	825	900	40	80	820

SPEED LIMIT (prior to construction)	SHIFTING TAPER LENGTH (1/2)L						STANDARD DEVICE SPACING IN FEET		BUFFER SPACE
	Lane Shift (FT)						Along Taper	Along Tangent	
MPH	2	4	6	8	10	12			FT
25	11	21	32	42	52	63	20	40	155
30	15	30	45	60	75	90	30	60	200
35	21	41	62	82	102	123	35	70	250
40	27	54	80	107	134	160	40	80	305
45	45	90	135	180	225	270	40	80	360
50	50	100	150	200	250	300	40	80	425
55	55	110	165	220	275	330	40	80	495
60	60	120	180	240	300	360	40	80	570
65	65	130	195	260	325	390	40	80	645
70	70	140	210	280	350	420	40	80	730
75	75	150	225	300	375	450	40	80	820

SPEED LIMIT (prior to construction)	SHOULDER TAPER LENGTH (1/3)L						STANDARD DEVICE SPACING IN FEET		BUFFER SPACE
	Shoulder Width (FT)						Along Taper	Along Tangent	
MPH	2	4	6	8	10	12			FT
25	7	14	21	28	35	42	20	40	155
30	10	20	30	40	50	60	30	60	200
35	14	28	41	55	68	82	35	70	250
40	18	36	54	72	89	107	40	80	305
45	30	60	90	120	150	180	40	80	360
50	34	67	100	134	167	200	40	80	425
55	37	74	110	147	184	220	40	80	495
60	40	80	120	160	200	240	40	80	570
65	44	87	130	174	217	260	40	80	645
70	47	94	140	187	234	280	40	80	730
75	50	100	150	200	250	300	40	80	820

- See MUTCD for taper formulas.
- ALLOWABLE LAP SPLICE FOR U-CHANNEL POST**
U-Channel posts may be spliced where long lengths are required. The upper section shall overlap the lower section by at least 24 inches. The bottom edge of the upper section of the splice shall be a minimum of 24 inches above the ground. The spliced sections shall be secured with at least four 5/16 inch diameter hex bolts spaced equally along the splice.



SHEET NUMBER: _____

DESIGN: B. BOUCHER, M. RILOVICH

CHECK: _____

DETAIL: _____

REVIEW: _____

DATE: Feb 7, 2025

APPROVED BY CHIEF ENGINEER: _____

PARISH: CADDO

SECTION: _____

STATE PROJECT: _____

DATE: 4-3-24

NO. _____

DESCRIPTION: _____

DOTD

LOUISIANA DEPARTMENT OF TRANSPORTATION & DEVELOPMENT

TRAFFIC ENGINEERING

TEMPORARY TRAFFIC CONTROL GENERAL NOTES SHEET

TTC-00 (C)

FILE NAME: A:\47000\47786\038\PW\CADD\Sheets\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2025, TIME: 5:06 PM, USER: ah5754

NO.	DATE	DESCRIPTION

DESIGNED BY: JB

DRAWN BY: JG

CHECKED BY: JB

halff

401 MARKET ST.
SUITE 650
SHREVEPORT, LA 71101
TEL: (318) 716-6136

JARED R. BOUGHENTS
REG. No. 41026
REGISTERED PROFESSIONAL ENGINEER
CIVIL ENGINEERING

2/17/2026

CITY OF SHREVEPORT
LOUISIANA

SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038

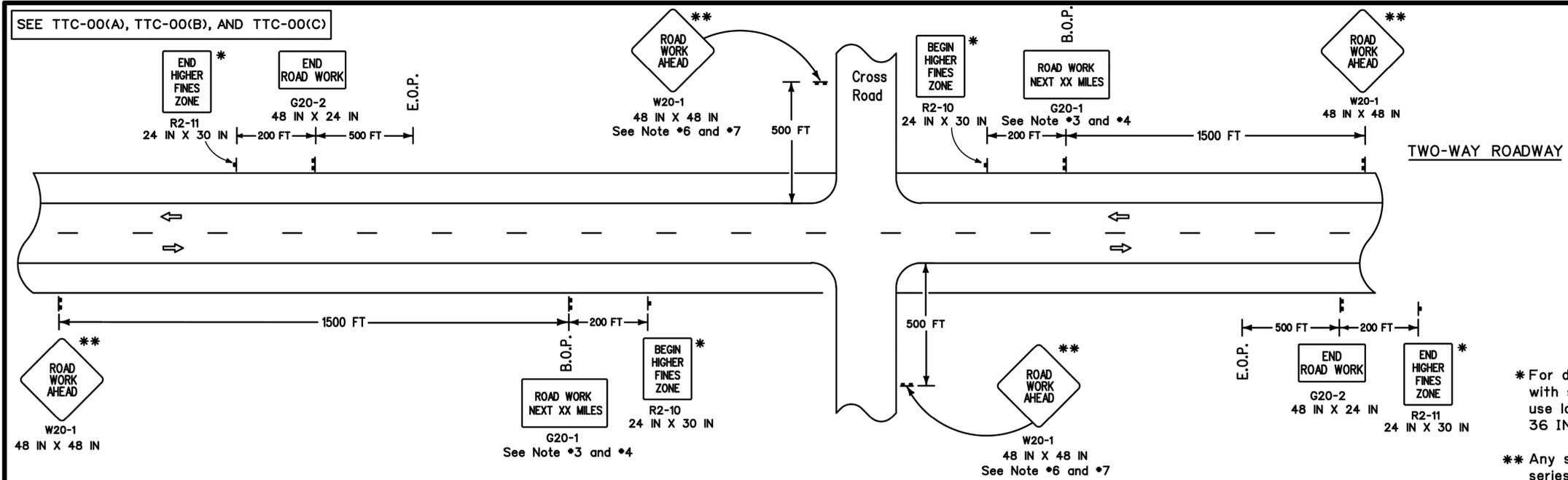
ISSUED: 2/17/26

SCALE: N.T.S.

SHEET TITLE: LADOTD STANDARD DETAILS (3 OF 6)

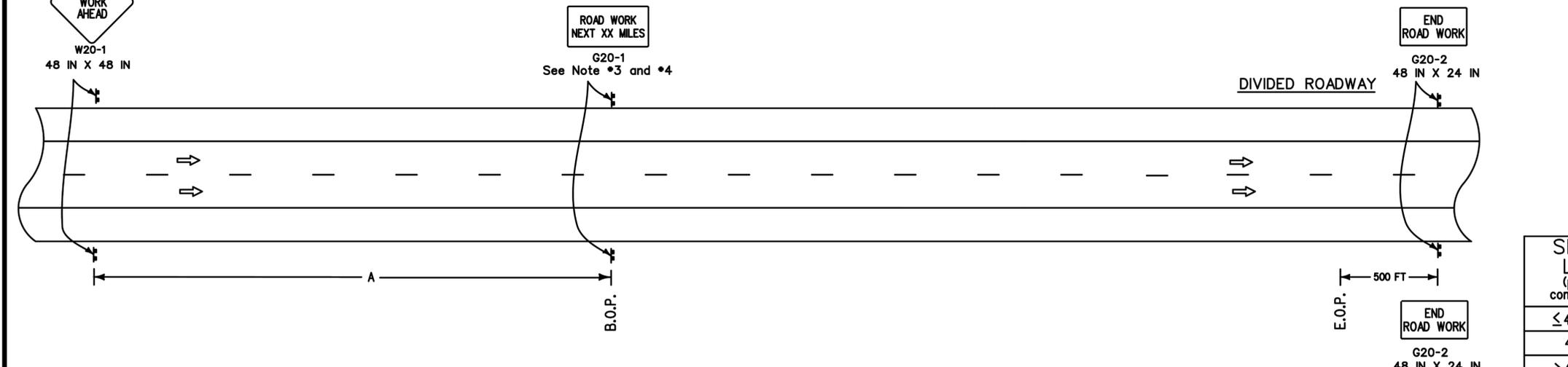
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NOTES
 This sheet shall be used with the Temporary Traffic Control General Notes Sheets TTC-00(A), TTC-00(B), TTC-00(C), and other Temporary Traffic Control Sheets as appropriate.

- This layout represents the minimum traffic controls required for placement of "Road Work Next XX Miles" and "End Road Work" signs.
- This layout does not replace other TTC Standard Sheets, but is intended as a supplement to the required signing.
- The distance on the "Road Work Next XX Miles" sign shall be rounded up to the nearest whole mile. This sign shall be placed at the Beginning of Project (B.O.P.) limits. This sign may be omitted if work zone is less than 0.5 miles.
- The "Road Work Next XX Miles" sign shall be a minimum of 60 inches by 36 inches for all multi-lane roadways and a minimum of 48 inches by 24 inches for two-lane roadways unless otherwise noted.
- The "End Road Work" sign shall be placed 500 feet past the End of Project (E.O.P.) limits.
- If "Road Work Ahead" sign is used on a cross road to warn of road work on another route, then "End Road Work" sign is not required.
- When projects are separated by less than 1 mile, they shall be signed as one project; this may require coordination.



LEGEND
 [Symbol] Traffic Sign
 [Arrow] Direction of Travel

TABLE

SPEED LIMIT (prior to construction)	SPACING 'A'
≤ 40 mph	1500 FT
45 mph	2640 FT
> 45 mph	5280 FT

* Sign spacing to be adjusted for Horizontal and Vertical curves.
 * For work outside of the traveled way, see TTC-01 and TTC-02.

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 ALL SITUATIONS SHALL BE REVIEWED AND/OR DESIGNED BY THE ENGINEER.
 CONTRACTORS ARE RESPONSIBLE FOR COMPLYING WITH ALL TTC STANDARDS.

SHEET NUMBER		PARISH		CONTROL SECTION		STATE PROJECT	
DESIGNER: B. BOUCHER		CHECKER: M. RILOVICH		CHECKER:		REVIEWER:	
APPROVED BY: [Signature]		DATE: Feb. 7, 2025		BRB BY:		REVISION OR CHANGE ORDER DESCRIPTION	
NO.		DATE		4-3-24		NO.	
 TEMPORARY TRAFFIC CONTROL LAYOUT FOR PLACEMENT OF ROAD WORK NEXT XX MILES AND END ROAD WORK SIGNS TTC-00 (D)							
 LOUISIANA DEPARTMENT OF TRANSPORTATION & DEVELOPMENT TRAFFIC ENGINEERING							

REVISION NO.	DATE	DESCRIPTION

DESIGNED BY: JB
 DRAWN BY: JG
 CHECKED BY: JB

401 MARKET ST.
 SUITE 650
 SHREVEPORT, LA 71101
 TEL. (318) 716-6136

JARED R. BOOGHEARTS
 REG. NO. 41026
 REGISTERED PROFESSIONAL ENGINEER
 CIVIL ENGINEERING

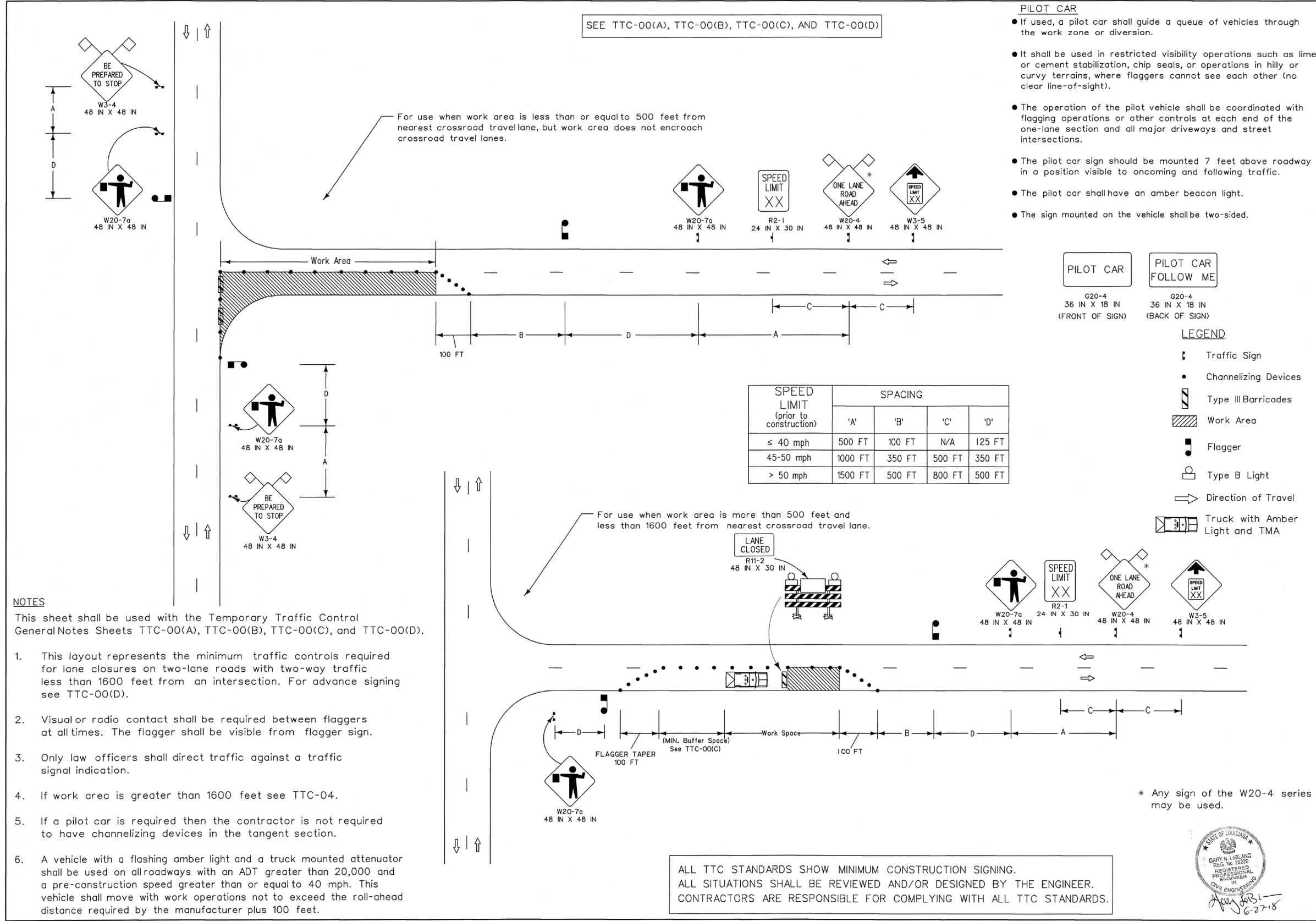
2/17/2026

CITY OF SHREVEPORT
 LOUISIANA

SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
 ISSUED: 2/17/26
 SCALE: N.T.S.
 SHEET TITLE
 LADOTD STANDARD
 DETAILS (4 OF 6)
 C10.16

FILE NAME: A:\47000us47786\038\PM\CADD\Sheets\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2026, TIME: 5:06 PM, USER: ah5754



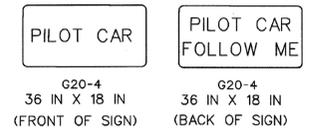
SEE TTC-00(A), TTC-00(B), TTC-00(C), AND TTC-00(D)

For use when work area is less than or equal to 500 feet from nearest crossroad travel lane, but work area does not encroach crossroad travel lanes.

For use when work area is more than 500 feet and less than 1600 feet from nearest crossroad travel lane.

SPEED LIMIT (prior to construction)	SPACING			
	'A'	'B'	'C'	'D'
≤ 40 mph	500 FT	100 FT	N/A	125 FT
45-50 mph	1000 FT	350 FT	500 FT	350 FT
> 50 mph	1500 FT	500 FT	800 FT	500 FT

- PILOT CAR**
- If used, a pilot car shall guide a queue of vehicles through the work zone or diversion.
 - It shall be used in restricted visibility operations such as lime or cement stabilization, chip seals, or operations in hilly or curvy terrains, where flaggers cannot see each other (no clear line-of-sight).
 - The operation of the pilot vehicle shall be coordinated with flagging operations or other controls at each end of the one-lane section and all major driveways and street intersections.
 - The pilot car sign should be mounted 7 feet above roadway in a position visible to oncoming and following traffic.
 - The pilot car shall have an amber beacon light.
 - The sign mounted on the vehicle shall be two-sided.



- LEGEND**
- ⬇ Traffic Sign
 - Channelizing Devices
 - ▨ Type III Barricades
 - ▨ Work Area
 - ⬆ Flagger
 - ⬆ Type B Light
 - ➡ Direction of Travel
 - 🚚 Truck with Amber Light and TMA

- NOTES**
- This sheet shall be used with the Temporary Traffic Control GeneralNotes Sheets TTC-00(A), TTC-00(B), TTC-00(C), and TTC-00(D).
1. This layout represents the minimum traffic controls required for lane closures on two-lane roads with two-way traffic less than 1600 feet from an intersection. For advance signing see TTC-00(D).
 2. Visual or radio contact shall be required between flaggers at all times. The flagger shall be visible from flagger sign.
 3. Only law officers shall direct traffic against a traffic signal indication.
 4. If work area is greater than 1600 feet see TTC-04.
 5. If a pilot car is required then the contractor is not required to have channelizing devices in the tangent section.
 6. A vehicle with a flashing amber light and a truck mounted attenuator shall be used on all roadways with an ADT greater than 20,000 and a pre-construction speed greater than or equal to 40 mph. This vehicle shall move with work operations not to exceed the roll-ahead distance required by the manufacturer plus 100 feet.

ALL TTC STANDARDS SHOW MINIMUM CONSTRUCTION SIGNING.
ALL SITUATIONS SHALL BE REVIEWED AND/OR DESIGNED BY THE ENGINEER.
CONTRACTORS ARE RESPONSIBLE FOR COMPLYING WITH ALL TTC STANDARDS.

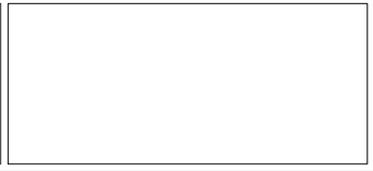


DESIGNED BY: G. LEBLANC	PARISH: CADDO	CONTROL SECTION: STATE	PROJECT: SOUTHFIELD ROAD IMPROVEMENTS
CHECKED BY: J. COLVIN	PROJECT NO.: 47786.038	DATE: 2/17/26	SCALE: N.T.S.
DATE: 2/17/26	REVISION OR CHANGE ORDER DESCRIPTION:	BY: JB	SHEET TITLE: LADOTD STANDARD DETAILS (5 OF 6)
NO. DATE	REVISION OR CHANGE ORDER DESCRIPTION:	DATE: 7/2/18	C10.17
APPROVED BY: GARY N. LEBLANC	CHIEF ENGINEER:	DATE: 6-27-18	

REVISION NO.	DATE	DESCRIPTION

DESIGNED BY: JB
DRAWN BY: JG
CHECKED BY: JB

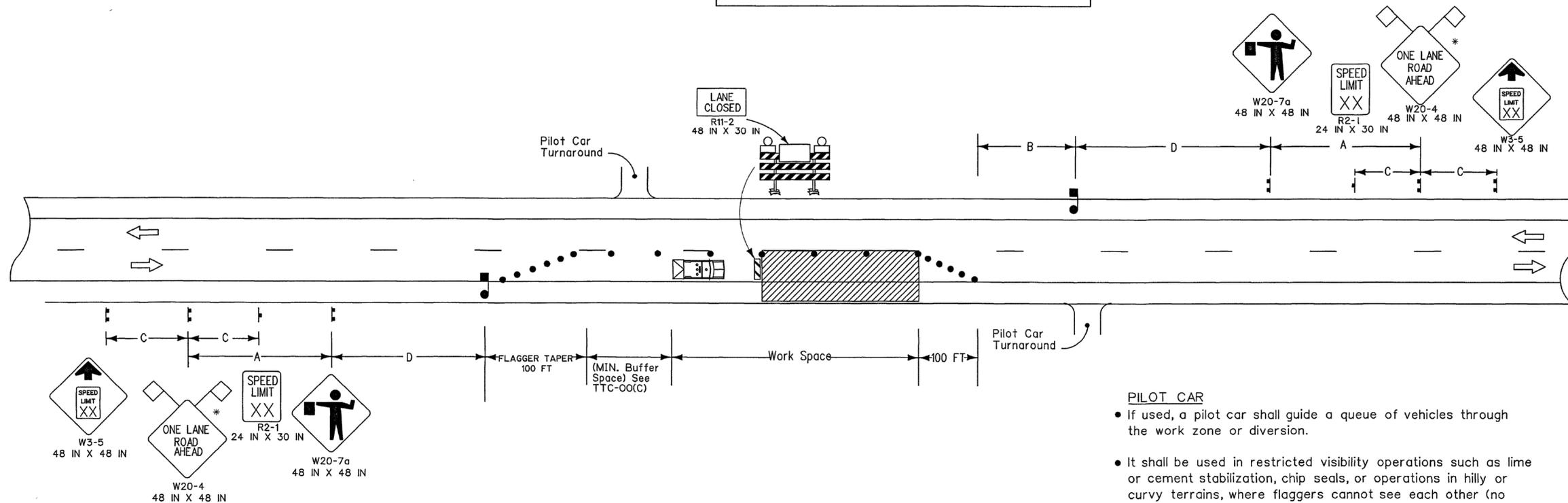
DATE: 2/17/2026



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/26
SCALE: N.T.S.
SHEET TITLE: LADOTD STANDARD DETAILS (5 OF 6)
C10.17

SEE TTC-00(A), TTC-00(B), TTC-00(C), AND TTC-00(D)



NOTES

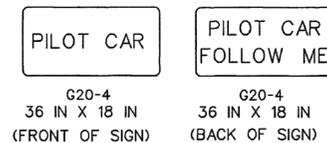
This sheet shall be used with the Temporary Traffic Control General Notes Sheets TTC-00(A), TTC-00(B), TTC-00(C) and TTC-00(D).

- This layout represents the minimum traffic controls required for lane closures on two-lane roads with two-way traffic greater than 1600 feet from an intersection. For this type of closure either a flagger or a pilot car will be required. For advance signing see TTC-00(D).
- To prevent vehicles from entering the work area against the flow of traffic, an additional flagger shall be stationed at each intersection, major driveway, railroad crossing, or crossing within the work area.
- For projects in rural areas the distance between flaggers shall not exceed:
 - (A) 2.5 miles for ADT < 2,500
 - (B) 2.0 miles for 2,500 < ADT < 5,000
 - (C) 1.5 miles for ADT > 5,000
- The flagger station shall be near the beginning of the taper and shall have adequate sight distance to be visible to oncoming traffic. If sight distance cannot be achieved, the distance between flaggers may be extended for a short duration.
- Visual or radio contact shall be required between flaggers at all times. The flagger shall be visible from the flagger sign.
- A vehicle with a flashing amber light and a truck mounted attenuator shall be used on all roadways with an ADT greater than 20,000 and a pre-construction speed greater than or equal to 40 mph. This vehicle shall move with work operations not to exceed the roll-ahead distance required by the manufacturer plus 100 feet.

- If a pilot car is required then the contractor is not required to have channelizing devices in the tangent section.
- If work zone is less than 1600 feet from an intersection see TTC-03.

PILOT CAR

- If used, a pilot car shall guide a queue of vehicles through the work zone or diversion.
- It shall be used in restricted visibility operations such as lime or cement stabilization, chip seals, or operations in hilly or curvy terrains, where flaggers cannot see each other (no clear line-of-sight).
- The operation of the pilot vehicle shall be coordinated with flagging operations or other controls at each end of the one-lane section and all major driveways and street intersections.
- The pilot car sign should be mounted 7 feet above roadway in a position visible to oncoming and following traffic.
- The pilot car shall have an amber beacon light.
- The sign mounted on the vehicle shall be two-sided.



SPEED LIMIT (prior to construction)	SPACING			
	'A'	'B'	'C'	'D'
≤ 40 mph	500 FT	100 FT	N/A	125 FT
45-50 mph	1000 FT	350 FT	500 FT	350 FT
≥ 55 mph	1500 FT	500 FT	800 FT	500 FT

* Any sign of the W20-4 series may be used.

LEGEND

- Traffic Sign
- Flagger
- Channelizing Devices
- Type III Barricades
- Work Area
- Type B Light
- Direction of Travel
- Truck with Amber Light and TMA



ALL TTC STANDARDS SHOW MINIMUM CONSTRUCTION SIGNING.
ALL SITUATIONS SHALL BE REVIEWED AND/OR DESIGNED BY THE ENGINEER.
CONTRACTORS ARE RESPONSIBLE FOR COMPLYING WITH ALL TTC STANDARDS.

SHEET NUMBER	93
PARISH	CADDO
CONTROL SECTION	000-09
STATE PROJECT	H.O.14410
DESIGNED BY	G. LEBLANC
CHECKED BY	J. COLVIN
DRAWN BY	C. FAKOURT
DATE	7/4/18
REVISION OR CHANGE ORDER DESCRIPTION	
APPROVED BY	<i>[Signature]</i>
CHIEF ENGINEER	
TEMPORARY TRAFFIC CONTROL LAYOUT FOR LANE CLOSURES ON TWO LANE ROADS WITH TWO-WAY TRAFFIC (FLAGGING OPERATIONS)	
TTC-04	

FILE NAME: A:\47000\47786\038\PW\CADD\Sheets\C-PLAN-C700-DTL-47786-038.dwg DATE: February 17, 2026, TIME: 5:06 PM, USER: ah5754

REVISION NO.	DATE	DESCRIPTION

DESIGNED BY: JB
DRAWN BY: JG
CHECKED BY: JB

401 MARKET ST.
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SHREVEPORT, LA 71101
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2/17/2026



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 2/17/26
SCALE: N.T.S.
SHEET TITLE
LADOTD STANDARD
DETAILS (6 OF 6)
C10.18

ELECTRICAL GENERAL NOTES:

- A. UPON COMPLETION, THE CONTRACTOR SHALL FURNISH A COMPLETE AND OPERABLE SYSTEM.
- B. CONTRACTOR SHALL COORDINATE WITH THE UTILITY COMPANY PRIOR TO CONSTRUCTION, TO VERIFY LOCATION AND ALL CONTRACTOR REQUIREMENTS FOR ESTABLISHING THE NEW ELECTRICAL SERVICE.
- C. LOUISIANA ONE CALL NOTIFICATION PROGRAM: IN ORDER TO FACILITATE ITS PURPOSE, L.R.S. 40:1749.13 STATES THAT NO PERSON SHALL EXCAVATE OR DEMOLISH WITHOUT FIRST ASCERTAINING THE LOCATION OF UNDERGROUND UTILITIES BY SERVING TELEPHONIC NOTICE TO A REGIONAL NOTIFICATION PROGRAM. IN THE STATE OF LOUISIANA, THE REGIONAL NOTIFICATION PROGRAM IS LOUISIANA ONE CALL. IN ORDER TO SERVE NOTICE OF EVACUATION, THIS PROGRAM CAN BE REACHED BY CALLING 1-800-272-3020.
- D. CAUTION: ALL UTILITIES ARE PLOTTED BASED ON INFORMATION FROM THE RESPECTIVE UTILITY COMPANIES. FIELD VERIFICATION IS REQUIRED FOR EXACT LOCATIONS. NOTE ROADWAY CROSSINGS OF OVERHEAD ELECTRICAL LINES. ANY OVERHEAD LINES SHOWN ON THE PLAN MUST BE IMMEDIATELY BROUGHT TO THE ENGINEER'S ATTENTION FOR VERIFICATION OF LIGHT POLE LOCATIONS.
- E. THESE DRAWINGS DO NOT INCLUDE NECESSARY COMPONENTS FOR CONSTRUCTION SAFETY.
- F. EQUIPMENT AND MATERIAL SHALL NOT BE ORDERED OR INSTALLED UNTIL APPROVED BY THE ENGINEER. APPROVAL DOES NOT RELIEVE THE CONTRACTOR FROM CONFORMANCE WITH THE CONTRACT, EXTEND TO QUANTITIES OR DIMENSIONS, IMPLY THAT EQUIPMENT CAN BE INSTALLED OR WILL OPERATE SATISFACTORILY, THE EQUIPMENT CONTAINS ALL NECESSARY COMPONENTS, OR THAT WILL COORDINATE WITH OTHER APPROVED ITEMS.
- G. MINIMUM SIZE CONDUIT SHALL BE 1 1/2". MINIMUM CONDUCTOR SIZE SHALL BE #10 AWG. MAXIMUM CONDUIT FILL FOR CONDUCTORS IN CONDUIT SHALL BE 25% IN LIEU OF THE 40% MAXIMUM FILL ALLOWED BY THE NATIONAL ELECTRICAL CODE (N.E.C.). PROVIDE WORKSPACE CLEARANCE FOR ALL ELECTRICAL EQUIPMENT ACCORDING TO N.E.C.
- H. IN ANY CASE WHERE THE DESIGN HEREIN DIFFERS FROM THE MINIMUM REQUIREMENTS SET DOWN BY THE N.E.C., THE CONTRACTOR SHALL MAINTAIN THE HIGHER LEVEL.
- I. ARC FLASH: ANY ENCLOSURE CONTAINING ENERGIZED ELECTRICAL EQUIPMENT SHALL BE FIELD MARKED IN ACCORDANCE WITH N.E.C. 110.16 TO WARN QUALIFIED PERSONS OF THE POTENTIAL OF ELECTRIC ARC FLASH HAZARD.
- J. EQUIPMENT LAYOUTS ARE DIAGRAMMATIC. THEY DO NOT SHOW EXACT LOCATIONS. EXACT LOCATIONS OF LIGHT POLES, CONDUITS, JUNCTION BOXES, ETC., SHALL BE AS DIRECTED BY THE PROJECT ENGINEER. THE CONTRACTOR SHALL FURNISH A COMPLETE SET OF "AS-BUILT" DRAWINGS TO THE ENGINEER INDICATING THE FINAL INSTALLED LOCATIONS OF ALL LIGHT POLES, FIXTURES, ABOVE AND UNDERGROUND CONDUITS, WIRING, SERVICE POLES, CONTROLLERS, ROADWAY BORES, JUNCTION BOXES, PLAN OR FIELD CHANGES, REVISED QUANTITIES, AND ADDITIONAL NOTES.
- K. THE LAYOUT SHEETS ARE INTENDED ONLY FOR A GENERAL OVERVIEW OF THE WORK REQUIRED. OMISSION FROM THE LAYOUT SHEETS OF ANY ITEM SHOWN ELSEWHERE IN THE PLANS DOES NOT RELIEVE THE CONTRACTOR FROM THE RESPONSIBILITY FOR ANY ASSOCIATED WORK.
- L. THE PLANS DO NOT NECESSARILY SHOW ALL OF THE UNDERGROUND FACILITIES. PRIOR TO ANY EXCAVATION, THE CONTRACTOR SHALL VERIFY THE LOCATION FOR EVIDENCE OF UNDERGROUND FACILITIES. NEITHER THE CITY OF SHREVEPORT NOR ENGINEER WILL BE HELD RESPONSIBLE FOR DAMAGE TO UNDERGROUND FACILITIES CAUSED BY CONTRACTOR OPERATIONS. IN ADDITION TO OTHER SOURCES, THE CONTRACTOR SHALL CONTACT THE LADOTD DISTRICT UTILITIES REPRESENTATIVE TO OBTAIN INFORMATION CONCERNING THE DEPARTMENT'S UNDERGROUND UTILITIES. CONTACT INFORMATION MAY BE OBTAINED FROM THE PROJECT ENGINEER. RESPONSIBILITY FOR DAMAGES AND WORKPLACE SAFETY REMAINS WITH CONTRACTOR.
- M. THE CONTRACTOR SHALL CONTACT THE CITY OF SHREVEPORT AND THE ELECTRICAL UTILITY COMPANY REPRESENTATIVE IN ORDER TO CREATE NEW ACCOUNT AND COORDINATE THE INSTALLATION OF THE REQUIRED ELECTRICAL SERVICE POINT(S) AND LIGHTING CONTROLLERS(S). TRANSFORMERS FOR EACH SERVICE POINT SHALL BE CENTER-TAP, 480 / 240V, 1 PH AS INDICATED. SEE DETAIL SHEETS FOR SERVICE POINT. PRIOR TO PERFORMING ANY CONSTRUCTION OR SERVICE TIE-INS, THE CONTRACTOR SHALL VERIFY ALL NEW AND EXISTING ELECTRICAL SERVICE AT THE PROJECT SITE WITH THE ELECTRICAL UTILITY COMPANY REPRESENTATIVE.
- N. ELECTRICAL DETAILS ARE TO ASSIST THE CONTRACTOR WITH BIDDING AND EQUIPMENT INSTALLATION. FOR SOME EQUIPMENT, MORE THAN ONE ELECTRICAL DETAIL MAY BE REFERENCED OR REQUIRED.
- O. FOR EACH ROADWAY LIGHTING INSTALLATION, THE CONTRACTOR SHALL INCLUDE, BUT NOT LIMITED TO, A PHOTOMETRIC REPORT/SUMMARY DESCRIBING LIGHT DISTRIBUTION, EFFICIENCY, ZONAL LUMEN OUTPUT, HORIZONTAL ILLUMINANCE, VEILING LUMINANCE (WHEN POLE HEIGHTS ARE LESS THAN 40 FEET), LUMINAIRE AND POLE CHARACTERISTICS, AND A DIAGRAM/DRAWING ILLUSTRATING FOOT-CANDLE LEVELS AT VARIOUS POINTS ALONG EACH ROADWAY SEGMENT. ADDITIONAL INFORMATION MAY BE REQUIRED AT THE DISCRETION OF THE ENGINEER. ALL REPORTS, DIAGRAMS, ETC. SHALL BE OF SUFFICIENT SIZE TO INCLUDE ALL THE NECESSARY INFORMATION.
- P. EXPOSED CONDUITS AND FITTINGS ON STRUCTURES SHALL BE THREADED SCHEDULE 40 PVC. CONDUITS ENTERING THE GROUND, CONCRETE SLABS, OR FOUNDATIONS SHALL BE SCHEDULE 40 PVC. ALL UNDERGROUND CONDUITS SHALL BE SCHEDULE 40 POLYETHYLENE OR PVC. UNDERGROUND CONDUITS SHALL BE ROUTED 4'-0" MINIMUM FROM SHOULDERS AND BURIED 3'-0" MINIMUM UNDERGROUND IN A 6" WIDE TRENCH, WITH THE EXCEPTION OF GALVANIZED RIGID STEEL SECTIONS, UNDERGROUND CONDUITS SHALL HAVE NO VERTICAL BENDS OR RUNS. NON-METALLIC CONDUITS SHALL NOT BE INSTALLED ABOVE GROUND OR SLABS.
- Q. ALL CONDUIT PLACED IN TRENCHES SHALL BE HAND PLACED IN THE TRENCH AND THE TRENCH BACKFILLED IN NO MORE THAN 12" LIFTS AND COMPACTED TO 95% MAXIMUM DENSITY ON THE SAME DAY. TRENCH SHALL NOT BE LEFT OVERNIGHT. WHEN POSSIBLE, MULTIPLE CONDUIT RUNS SHALL BE PLACED IN COMMON TRENCHES. WHEN CONDUITS ARE INSTALLED ACROSS ROADWAYS, DRIVEWAYS, ETC., CONDUITS SHALL BE INSTALLED IN A SCHEDULE 80 POLYETHYLENE OR PVC JACKED OR BORED PROTECTIVE CASING. CASING SIZE(S) SHALL BE AS INDICATED ON THE PLANS OR AS DIRECTED BY THE PROJECT ENGINEER. CASINGS SHALL BE LOCATED 4'-0" MINIMUM BELOW THE ROADWAY. EXCAVATION SHALL NOT TAKE PLACE CLOSER THAN 4'-0" FROM ANY SHOULDER EDGE AND WATER SHALL NOT BE USED IN THE JACKING PROCESS. SURFACE MARKERS SHALL BE USED TO MARK THE ENDS OF ROADWAY CASINGS.
- R. WHEN FLEXIBLE CONDUITS ARE REQUIRED, BX, MC, OR ARMORED CABLE SHALL NOT BE ALLOWED. FLEXIBLE CONDUITS SHALL BE LIQUID-TIGHT FLEXIBLE METALLIC CONDUIT (LFMC) CONFORMING TO N.E.C. AND BE UL LISTED AS PER UL 360. LFMC SHALL BE INSTALLED AS INDICATED ON THE PLANS OR AS DIRECTED BY THE PROJECT ENGINEER. LFMC SHALL BE CONSTRUCTED OF A HOT-DIPPED GALVANIZED, CONTINUOUS INTERLOCKING STEEL STRIP HAVING A HEAVY COATING OF ZINC HELICALLY FORMED INTO A METAL CORE. LFMC SHALL BE GRAY IN COLOR, HAVE A MOISTURE RESISTANT THERMOPLASTIC JACKET EXTRUDED OVER THE METAL CORE, SHALL BE OIL RESISTANT, AND UV SUNLIGHT RESISTANT. THE THERMOPLASTIC JACKET SHALL HAVE PRINTING IDENTIFYING MANUFACTURE'S NAME, PRODUCT NAME, SIZE, UL, CSA, AND SUNLIGHT AND OIL RESISTIVITY. LFMC SHALL ACCEPT STANDARD LIQUID-TIGHT METAL FITTINGS AND BE CONSTRUCTED SUCH THAT THE RACEWAY WILL SEAL OUT, BUT NOT LIMITED TO, WEATHER, WATER, LIQUIDS, ABRASIVES, ALCOHOL, COOLANTS, CORROSIVE FUMES, GASES, DIRT, GREASE, MINERAL ACIDS, NON-CONCENTRATED FIXED ALKALINES, PETROLEUM OILS, AND SALT AIR AND SPRAY. ALL LOCATIONS REQUIRING FLEXIBLE CONDUIT INSTALLATIONS SHALL HAVE FLEXIBLE LENGTHS OF 3'-0" (MAX.).
- S. UNLESS NOTED OTHERWISE IN THE PLANS, ALL CONDUCTORS SHALL BE INSTALLED IN RACEWAYS AND SHALL BE CLASS B STRANDED COPPER HAVING 600 VOLT RATED CROSS-LINKED POLYETHYLENE INSULATION CONFORMING TO INSULATED CABLE ENGINEERS ASSOCIATION (ICEA) STANDARD S-66-524, INSTITUTE OF ELECTRICAL AND ELECTRONICS ENGINEERS (IEEE) 1202, FEDERAL SPECIFICATION A-A-59544, NEMA PUBLICATION NO. WC-70, AND THE NATIONAL ELECTRIC CODE FOR CROSS-LINKED THERMOSETTING POLYETHYLENE INSULATED WIRE AND POWER CABLE. WHERE CONDUCTORS ARE CONNECTED TO, OR INSTALLED NEAR HEAT PRODUCING EQUIPMENT (LUMINARIES, HEATERS, MOTORS, ETC.), THE CONDUCTOR INSULATION FOR THE AFFECTED CONDUCTORS SHALL HAVE A TEMPERATURE RATING IN EXCESS OF THE TEMPERATURE EXPECTED TO BE ENCOUNTERED.
- T. UNLESS DIRECTED OTHERWISE BY THE ENGINEER, ALL UNDERGROUND NON-METALLIC CONDUITS SHALL CONTAIN A BARE SOLID COPPER GROUNDING CONDUCTOR. ALL OTHER CONDUITS SHALL HAVE GREEN INSULATED GROUNDING CONDUCTORS WITH THE INSULATION OF THE SAME TYPE AS THE PHASE CONDUCTORS.
- U. THE CONTRACTOR SHALL PROVIDE ALL NECESSARY CONNECTIONS TO ALL EQUIPMENT REQUIRING ELECTRICAL SERVICE. UNLESS INDICATED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE ENGINEER, ALL SPLICES, JOINTS, TAPS, AND CONNECTIONS SHALL BE MADE ACCESSIBLE AND ONLY IN JUNCTION BOXES OR EQUIPMENT ENCLOSURES. SPLICES OR TAPS ARE NOT PERMITTED IN CONDUIT BODIES OR RACEWAYS. UNLESS INDICATED OTHERWISE ON THE PLANS OR AS DIRECTED BY THE ENGINEER, SPLICES AND TAPS SHALL BE MADE WITH INSULATED COMPRESSION-TYPE CONNECTORS. SCREW-ON TYPE WIRE NUTS ARE NOT ALLOWED. SERVICE AND FEEDER CONDUCTORS SHALL BE INSTALLED IN THEIR ENTIRE LENGTH WITHOUT SPLICES. WHERE TAPS ARE REQUIRED FROM FEEDER OR SERVICE CONDUCTORS, TAPS SHALL BE MADE WITHOUT CUTTING THE MAIN CONDUCTORS. FEEDER OR SERVICE TAPS SHALL BE MADE WITH PARALLEL-TYPE GUTTER TAP CONNECTORS HAVING INSULATED COVERS. WHEN TERMINAL BLOCKS ARE REQUIRED, TERMINAL BLOCKS SHALL BE 600 VOLT, 350 AMP, POWER DISTRIBUTION TYPE, INTERMEDIATE SIZE, BOX TO BOX TYPE.
- V. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CHECKING WITH VARIOUS UTILITY COMPANIES TO DETERMINE AND/OR VERIFY THE EXACT LOCATION OF ALL UTILITIES PRIOR TO STARTING EXCAVATION. THE CONTRACTOR SHALL REPAIR ALL DAMAGE CAUSED BY HIS EMPLOYEES AND/OR EQUIPMENT TO ANY UTILITIES, TO THE SATISFACTION OF THE UTILITY COMPANY.
- W. ALL EQUIPMENT, MATERIAL AND APPARATUS SHALL BE AS SHOWN OR EQUAL.
- X. CONCRETE SHALL BE STRUCTURAL CLASS "S" AND AS INDICATED IN ACCORDANCE WITH THE PROJECT SPECIFICATIONS; 3000 PSI CONCRETE SHALL BE UTILIZED FOR THE CONSTRUCTION OF LIGHT POLE BASES AND OTHER STRUCTURES.

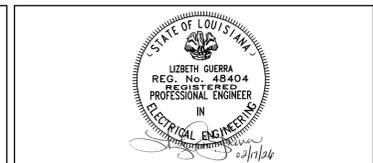
LIGHT FIXTURE SCHEDULE						
TAG	TYPE	MOUNTING	MANUFACTURER	LED	POLE	DESCRIPTION
A	LED ROADWAY DECORATIVE POST TOP LANTERN	LED POST TOP LANTERN TO BE MOUNTED ON TOP TENON OF CONCRETE POLE	HOLOPHANE: ARE3 P80 30K XVOLT GL3 BK SK L20 PHSS90 OR APPROVED EQUAL	LED 3000K 9,915LM 100W	AMERON: VEF06.1 20'-0"	480V, TYPE 3 DISTRIBUTION DECORATIVE LANTERN WITH HOUSE SIDE SHIELDING WITH SPIKE FINIAL, 20' DECORATIVE CONCRETE POLE WITH DOUBLE CABLE ENTRANCE FOR WIRING TO TOP TENON.

ELECTRICAL SERVICE								
SERVICE POINT	VOLTAGE	PHASE	NUMBER OF CIRCUITS	MAIN BREAKER	CIRCUIT BREAKER		CONTACTOR	SERVICE CONDUIT AND CONDUCTORS
					S1	S2		
A	240/480	1-PHASE	2	100A	2P/15A	2P/15A	30A 12-POLE	3#1, 1#8G IN 1-1/2" CONDUIT

FILE NAME: A:\47000s\47786\038\ELE\EC\CADD\Sheets\E001-CN-47786.038.dwg DATE: February 17, 2026, TIME: 11:09 AM, USER: ah5593

REVISION		
NO.	DATE	DESCRIPTION

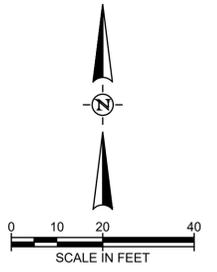
GC/LDLG
DESIGNED BY:
GC/LDLG
DRAWN BY:
LG
CHECKED BY:



SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 09/29/2025
SCALE: NONE
SHEET TITLE
ELECTRICAL GENERAL
NOTES & SCHEDULES
E001

FILE NAME: A:\4700\47786\038\ELECCADD\Sheets\E101-ES-47786.038.dwg DATE: February 6, 2026, TIME: 9:29 AM, USER: ah5593



LEGEND:

- PROPOSED STREET LIGHT
- PROPOSED UNDERGROUND CONDUIT
- SWEPco PEDESTAL
- STATION NUMBER & OFFSET
- SERVICE POINT-CIRCUIT NUMBER-POLE #
- EXISTING SWEPco POLE MOUNTED TRANSFORMER BANK

NOTES:

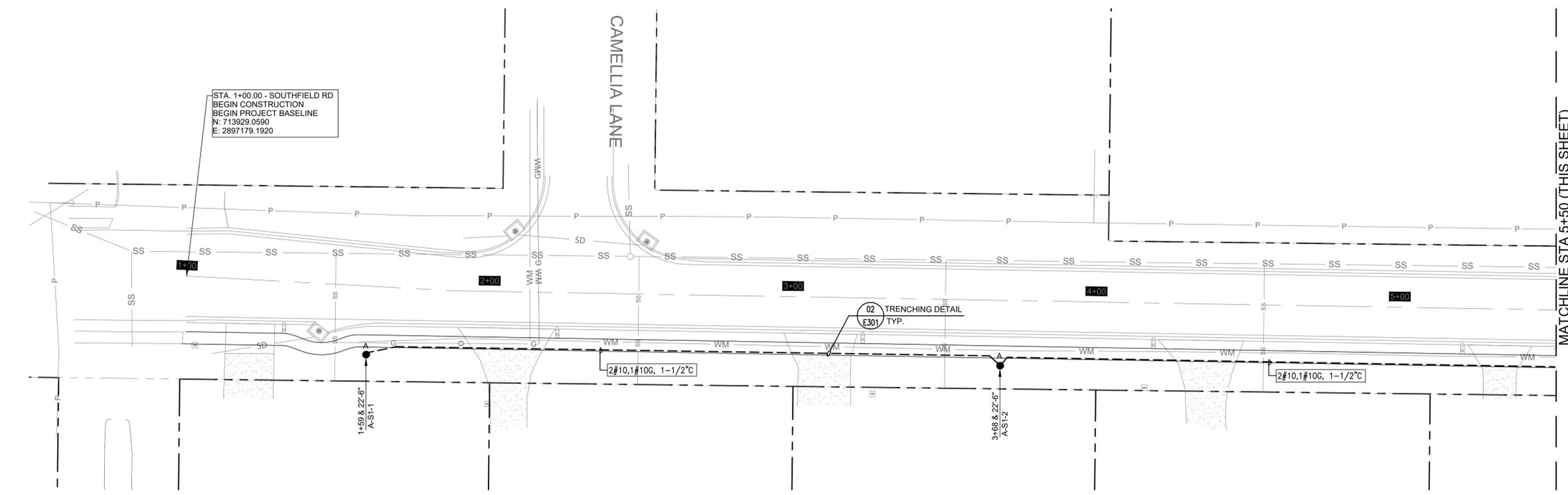
- A. COORDINATE EXACT LOCATION OF ALL UNDERGROUND UTILITIES WITH GENERAL CONTRACTOR PRIOR TO EXCAVATION, BORING, TRENCHING, OR DRILLING FOOTINGS FOR LUMINAIRES.
- B. ALL LUMINAIRES SHALL BE 10'-0" CLEAR FROM THE NEAREST OVERHEAD UTILITY LINES.
- C. CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS PRIOR TO CONDUIT INSTALLATION TO AVOID CONFLICTS WITH EXISTING WATER UTILITY LINES.
- D. CONTRACTOR SHALL CONTACT SWEPco/AEP UTILITY COMPANY SERVICE REPRESENTATIVE AND COORDINATE NEW SERVICE LOCATION PRIOR TO COMMENCING ANY WORK.

POINT OF CONTACT: COREY A. RANDOLPH
 PHONE: 318-401-8850
 EMAIL: CRANDOLPH@AEP.COM

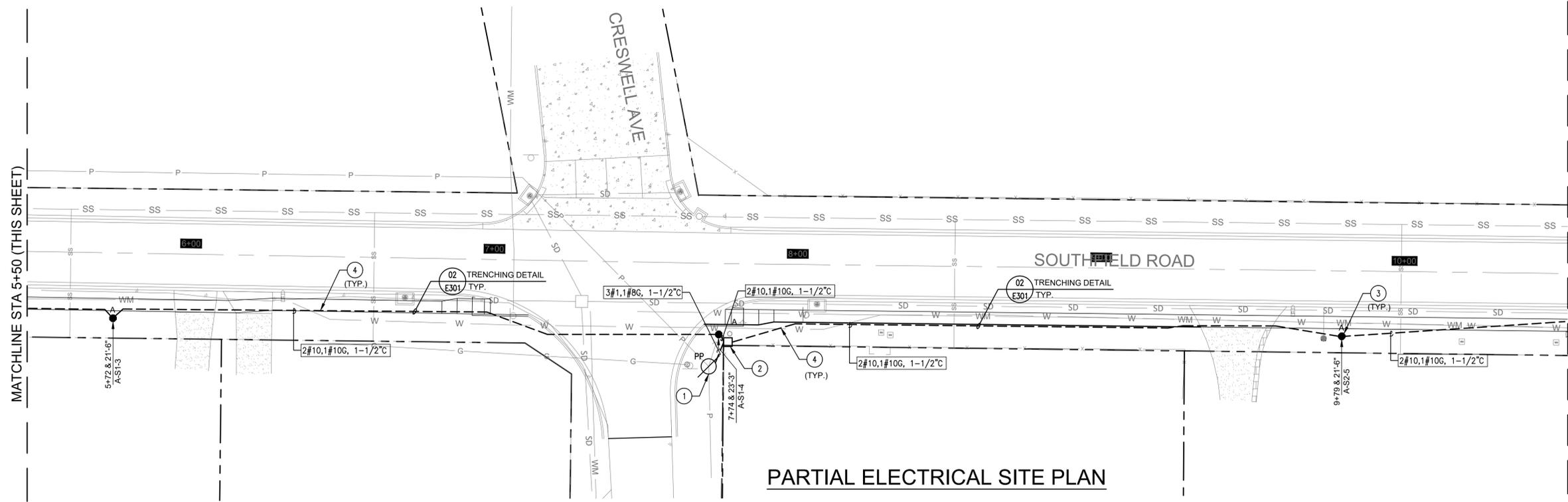
KEY NOTES:

1. ELECTRICAL SERVICE POINT A. CONTRACTOR SHALL COORDINATE ELECTRICAL SERVICE WITH SWEPco PRIOR TO COMMENCING WORK. REFER TO DETAIL 01 ON SHEET E304 FOR SERVICE DETAILS.
2. SWEPco PEDESTAL LIGHTING CONTROLLER. CONTRACTOR SHALL COORDINATE FINAL LOCATION WITH SWEPco PRIOR TO COMMENCING WORK. REFER TO DETAILS 02,03 AND 04 ON SHEET E304 FOR LIGHTING CONTROLLER CONNECTION AND MOUNTING DETAILS.
3. WHERE PROPOSED POLE LOCATIONS ARE FOUND TO CONFLICT WITH EXISTING WATER UTILITIES THE WATER UTILITIES ARE TO BE RELOCATED.
4. CONTRACTOR SHALL VERIFY THAT ALL REQUIRED VERTICAL AND HORIZONTAL CLEARANCES ARE MET WHERE NEW ELECTRICAL CONDUIT CROSSES OR PARALLELS EXISTING UTILITIES.

CAUTION!!!
 EXISTING OVERHEAD ELECTRIC LINES AND EXISTING UNDERGROUND UTILITIES IN AREA TO REMAIN. PROTECT DURING CONSTRUCTION.



PARTIAL ELECTRICAL SITE PLAN



PARTIAL ELECTRICAL SITE PLAN

NO.	DATE	DESCRIPTION

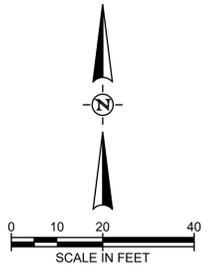
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SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
 ISSUED: 02/06/2026
 SCALE: AS NOTED
 SHEET TITLE
 ELECTRICAL SITE PLAN (1 OF 2)
 E101

FILE NAME: A:\4700\47786\038\ELEC\CADD\Sheets\E102-ES-47786.038.dwg DATE: February 6, 2026, TIME: 9:30 AM, USER: ah5593



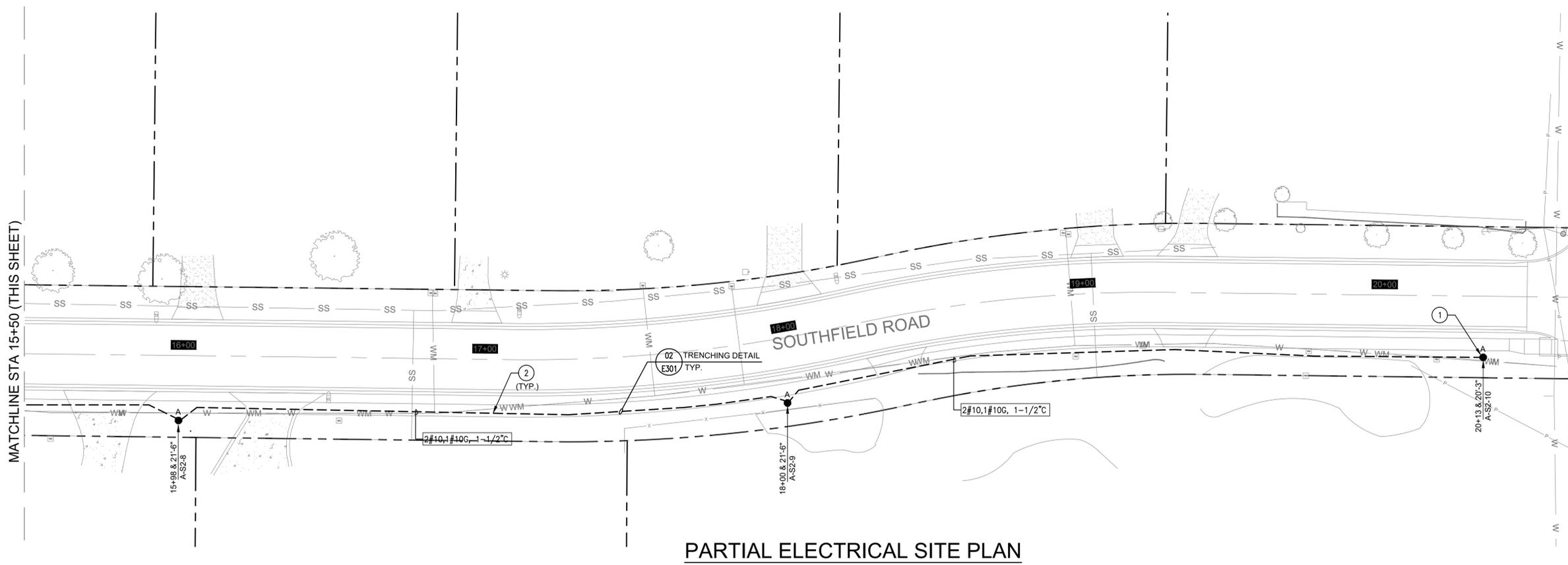
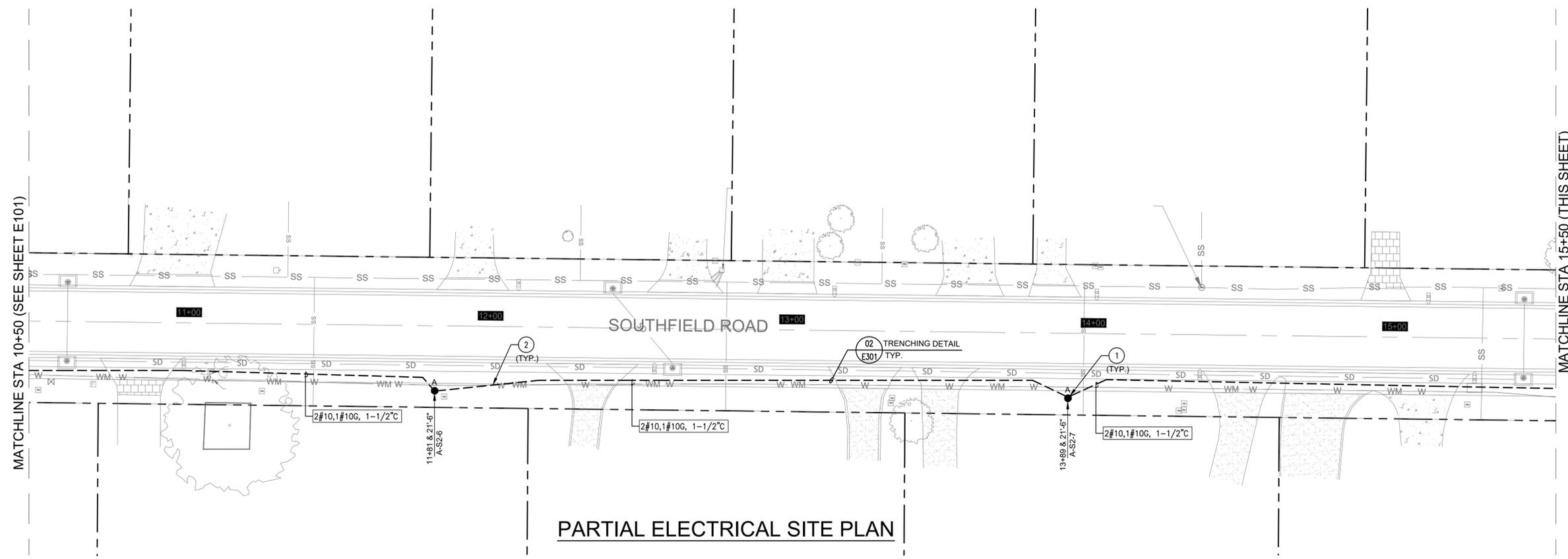
LEGEND:

	PROPOSED STREET LIGHT
	PROPOSED UNDERGROUND CONDUIT
	SWEPCO PEDESTAL
	STATION NUMBER & OFFSET SERVICE POINT-CIRCUIT NUMBER-POLE #
	EXISTING SWEPCO POLE MOUNTED TRANSFORMER BANK

- NOTES:**
- COORDINATE EXACT LOCATION OF ALL UNDERGROUND UTILITIES WITH GENERAL CONTRACTOR PRIOR TO EXCAVATION, BORING, TRENCHING, OR DRILLING FOOTINGS FOR LUMINAIRES.
 - ALL LUMINAIRES SHALL BE 10'-0" CLEAR FROM THE NEAREST OVERHEAD UTILITY LINES.
 - CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS PRIOR TO CONDUIT INSTALLATION TO AVOID CONFLICTS WITH EXISTING WATER UTILITY LINES.
 - CONTRACTOR SHALL CONTACT SWEPCO/AEP UTILITY COMPANY SERVICE REPRESENTATIVE AND COORDINATE NEW SERVICE LOCATION PRIOR TO COMMENCING ANY WORK.
- POINT OF CONTACT: COREY A. RANDOLPH
PHONE: 318-401-8850
EMAIL: CRANDOLPH@AEP.COM

- KEY NOTES:**
- WHERE PROPOSED POLE LOCATIONS ARE FOUND TO CONFLICT WITH EXISTING WATER UTILITIES THE WATER UTILITIES ARE TO BE RELOCATED.
 - CONTRACTOR SHALL VERIFY THAT ALL REQUIRED VERTICAL AND HORIZONTAL CLEARANCES ARE MET WHERE NEW ELECTRICAL CONDUIT CROSSES OR PARALLELS EXISTING UTILITIES.

CAUTION!!!
EXISTING OVERHEAD ELECTRIC LINES AND EXISTING UNDERGROUND UTILITIES IN AREA TO REMAIN. PROTECT DURING CONSTRUCTION.



REVISION		
NO.	DATE	DESCRIPTION

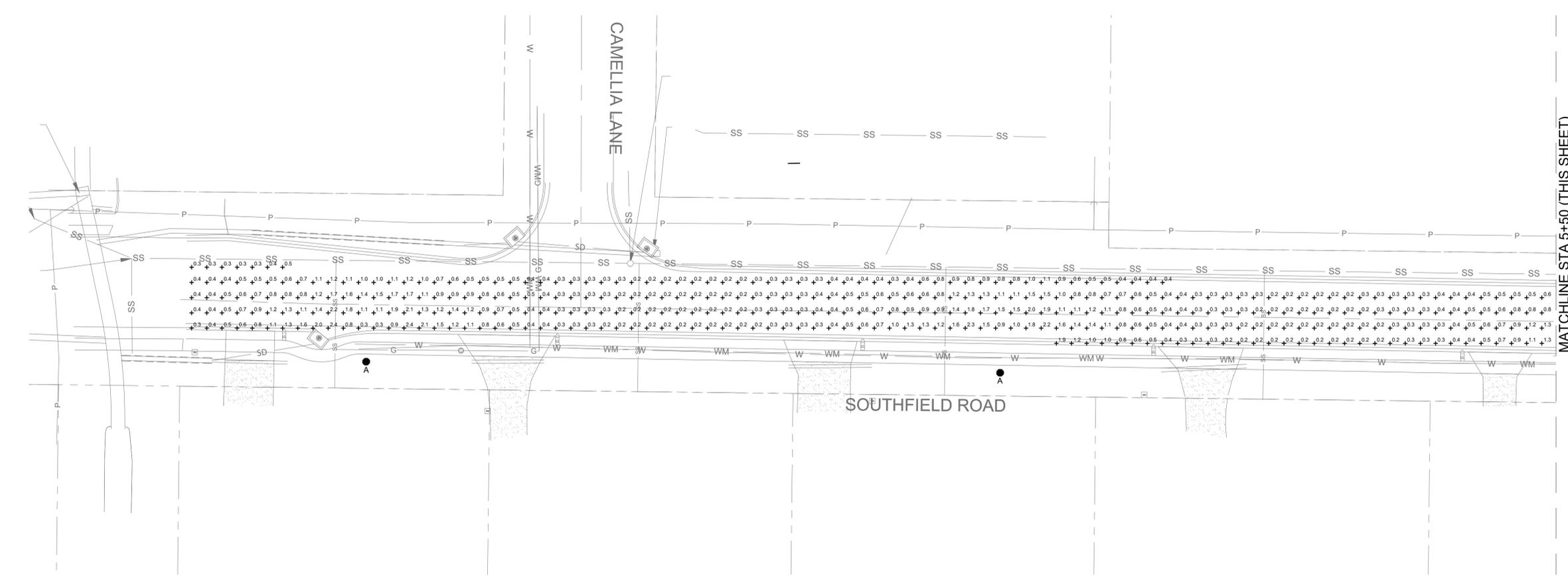
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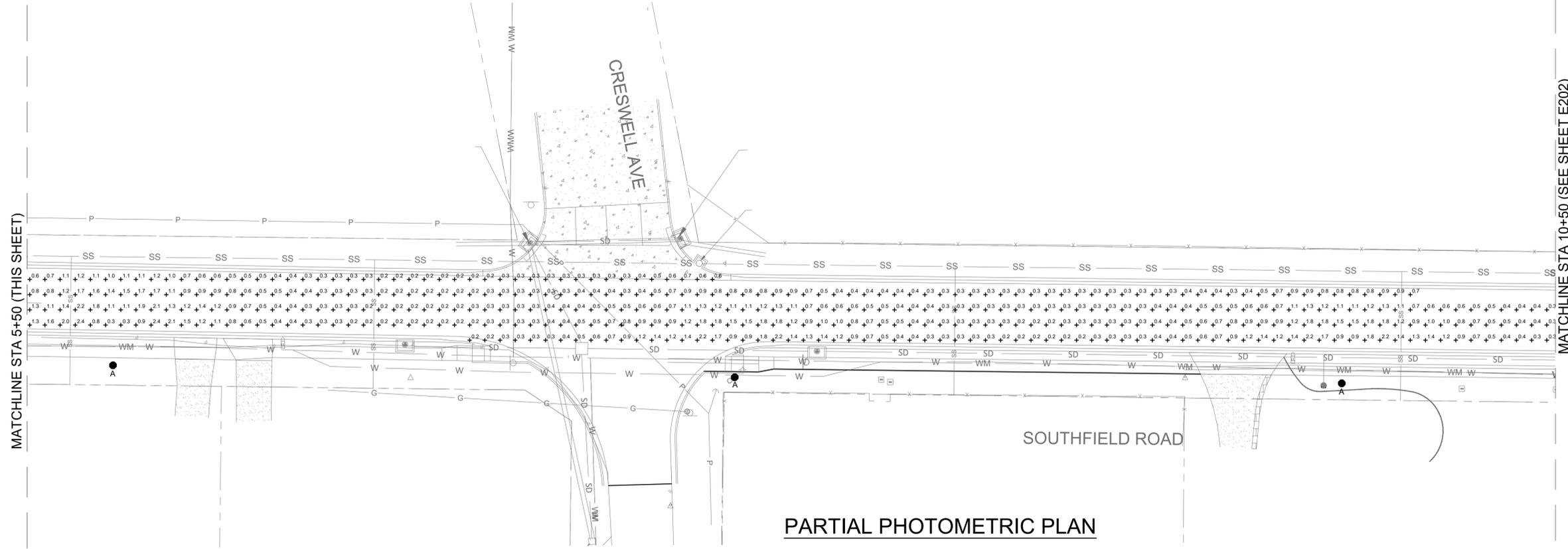
SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 02/06/2026
SCALE: AS NOTED
SHEET TITLE
ELECTRICAL SITE
PLAN (2 OF 2)
E102

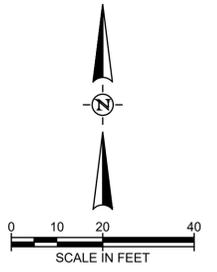
FILE NAME: A:\4700\47786\038\ELECCADD\Sheets\E201-ES-47786.038.dwg DATE: February 6, 2026, TIME: 9:30 AM, USER: ah5593



PARTIAL PHOTOMETRIC PLAN



PARTIAL PHOTOMETRIC PLAN



LEGEND:
 ● A PROPOSED STREET LIGHT

SHREVEPORT ORDINANCES	
MINIMUM FC @ CENTERLINE 0.2FC	MINIMUM AVG:MIN 4:1

LIGHTING PHOTOMETRICS			
AVG FC 0.7 FC	MIN FC 0.2 FC	MAX 2.4FC	AVG:MIN 3.5:1

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NO.	DATE	DESCRIPTION

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 LG
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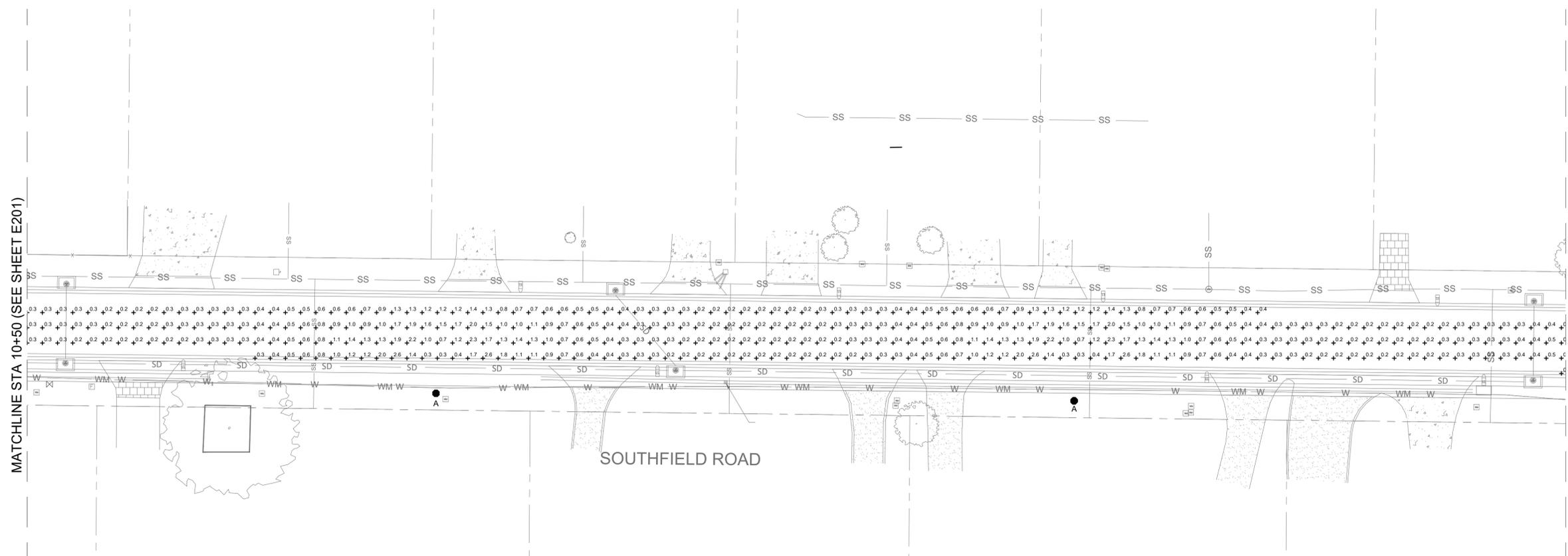
SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
 ISSUED: 02/06/2026
 SCALE: AS NOTED
 SHEET TITLE
 PHOTOMETRIC PLAN
 (1 OF 2)
 E201

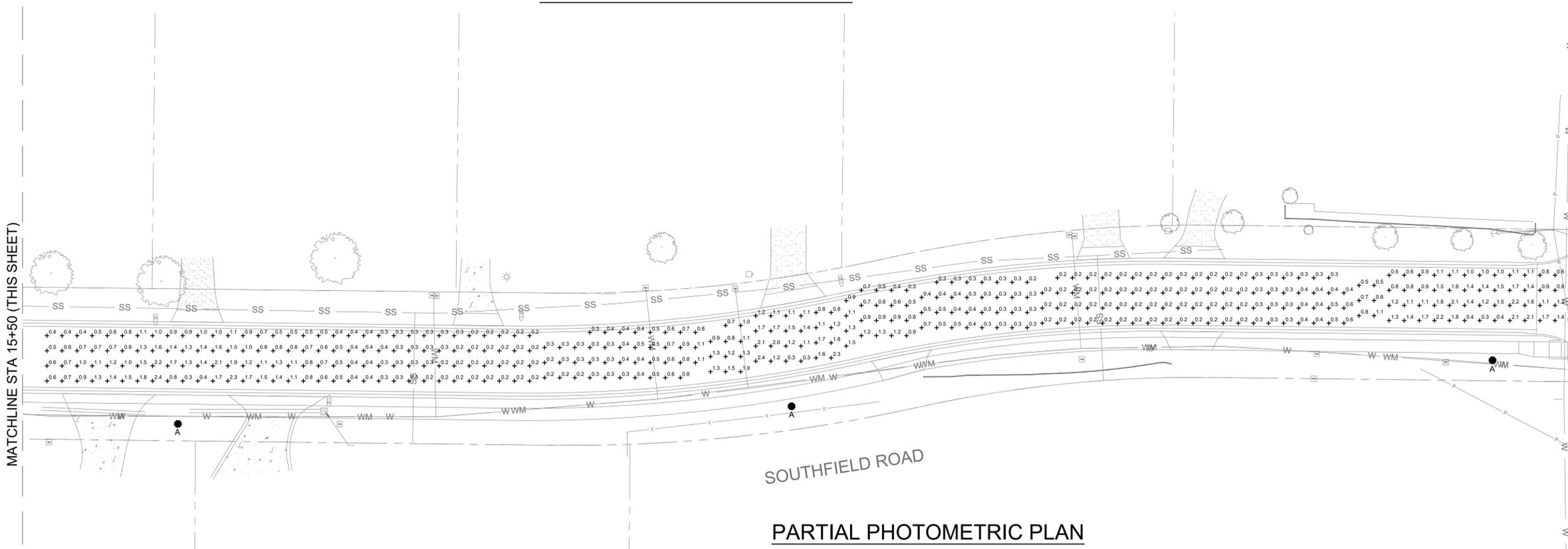
FILE NAME: A:\47000\47786\038\ELC\CADD\Sheets\E202-ES-47786.dwg DATE: February 6, 2026, TIME: 9:31 AM, USER: ah6593

MATCHLINE STA 10+50 (SEE SHEET E201)

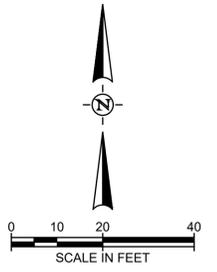
MATCHLINE STA 15+50 (THIS SHEET)



PARTIAL PHOTOMETRIC PLAN



PARTIAL PHOTOMETRIC PLAN



LEGEND:
 ● A PROPOSED STREET LIGHT

SHREVEPORT ORDINANCES			
MINIMUM FC @ CENTERLINE	0.2FC	MINIMUM AVG:MIN	4:1

LIGHTING PHOTOMETRICS			
AVG FC	MIN FC	MAX	AVG:MIN
0.7 FC	0.2 FC	2.4FC	3.5:1

REVISION		
NO.	DATE	DESCRIPTION

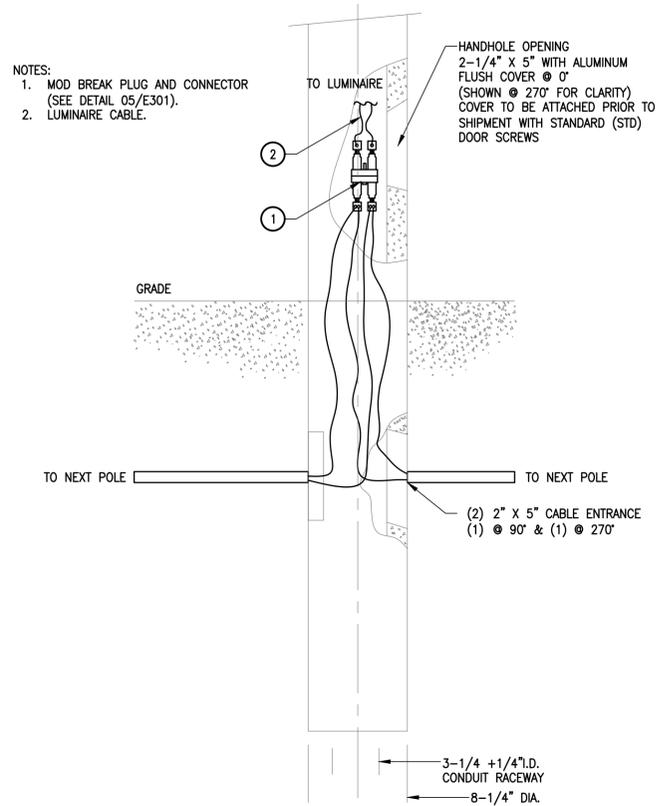
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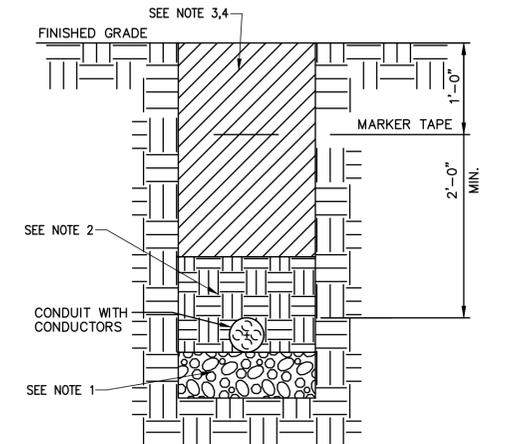
SOUTHFIELD ROAD IMPROVEMENTS
 CITY OF SHREVEPORT
 CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
 ISSUED: 02/06/2026
 SCALE: AS NOTED
 SHEET TITLE
 PHOTOMETRIC PLAN
 (2 OF 2)
 E202

FILE NAME: A:\4700\47786\038\ELEC\CADD\Sheets\E301-DT-47786-038.dwg DATE: February 6, 2026, TIME: 9:31 AM, USER: ah5593

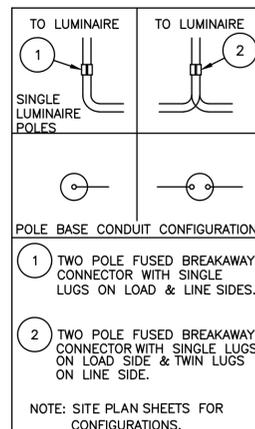


04 TYPICAL BREAKAWAY AND LIGHT POLE CONNECTIONS
N.T.S.

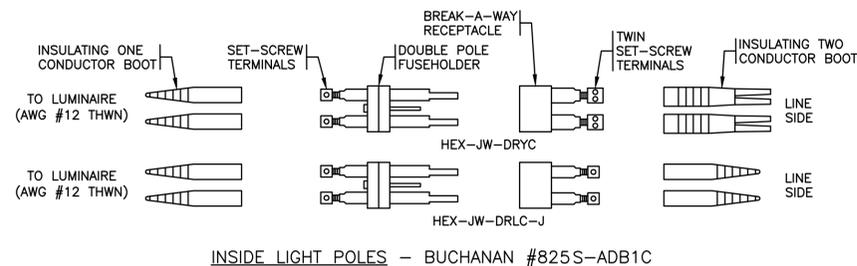


- NOTE:
- INITIAL BACKFILL AND BEDDING OF TAMPED GRANULAR MATERIAL
 - FINAL BACKFILL FREE OF LARGE STONES, ROCKS, CLUMPS, AND DEBRIS.
 - ANY ASPHALT OR CONCRETE MATERIAL REMOVED DURING TRENCHING SHALL BE REPLACED WITH LIKE MATERIAL TO MATCH EXISTING.
 - SEE PLAN SPECIFICATIONS, GENERAL NOTES, LAYOUT SHEETS, AND DETAILS FOR ADDITIONAL INFORMATION.

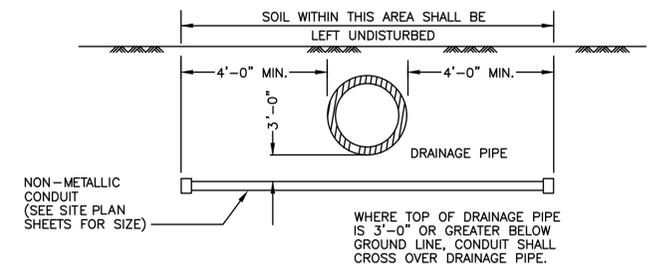
02 TRENCHING (TYPICAL)
N.T.S.



05 BREAKAWAY & TAP CONNECTOR DIAGRAMS
N.T.S.



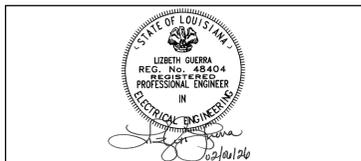
03 BREAKAWAY AND TAP CONNECTOR
N.T.S.



01 DRAINAGE PIPE CROSSING (TYPICAL)
N.T.S.

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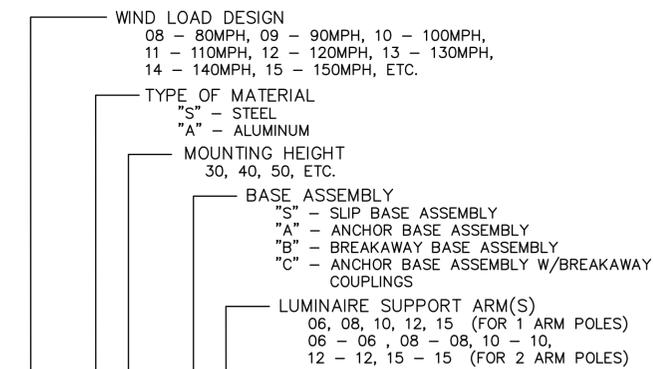
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CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 02/06/2026
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SHEET TITLE
ELECTRICAL DETAILS
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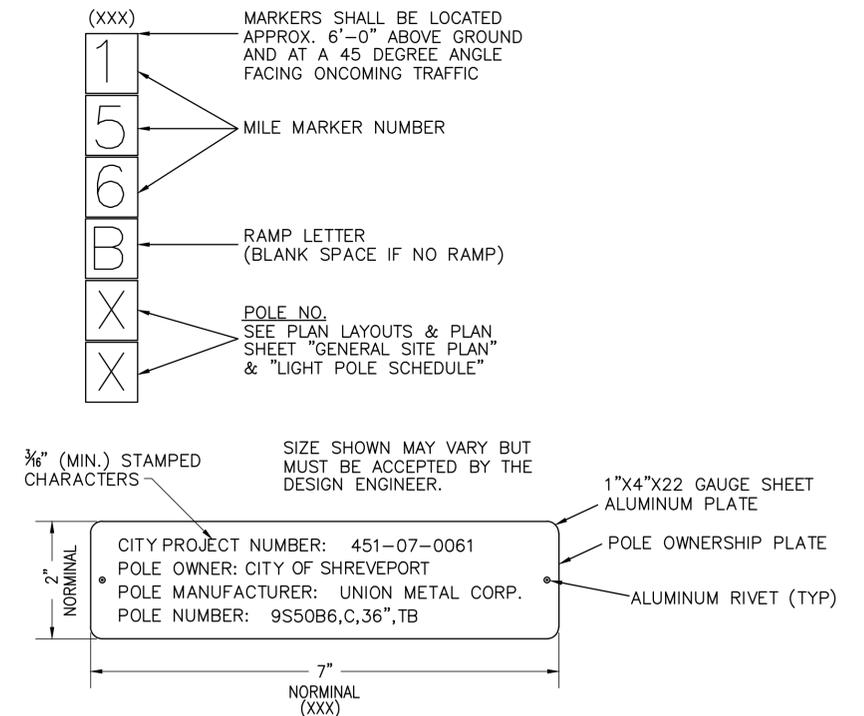
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ADDITIONAL SUFFIXES MAY BE AS FOLLOWS:

- "C" - BOLT COVERS
- "B" - SINGLE BALLAST ADAPTER
- "B2" - TWIN BALLAST ADAPTER
- "NH" - OMIT HANDHOLE
- "NAB" - OMIT ANCHOR BOLTS
- "P" - PAINTED POLE FOLLOWED BY COLOR
- "X" - HANDHOLE LOCATION OTHER THAN 90° (90° IS STANDARD LOCATION)
- "CA" - CLAMP TYPE ARM ATTACHMENT (FOR ALUMINUM POLES ONLY)
- "FI" - FULLY INTERCHANGEABLE POLES
- "TB" - TRANSFORMER BASE WITH POLE

02 AASHTO STANDARD POLE NUMBER FORMAT
 N.T.S.



01 POLE MARKER DECAL AND OWNERSHIP PLATE
 N.T.S.

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PROJECT NO.: 47786.038
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SHEET TITLE
ELECTRICAL DETAILS
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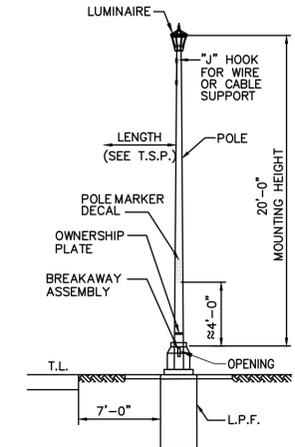
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SEE PLAN LAYOUT SHEETS FOR ADDITIONAL LUMINAIRE INFORMATION.

POLE AND HARDWARE SHALL CONFORM TO AASHTO-AGC-ARTBA PUBLICATION "A GUIDE TO STANDARDIZED HIGHWAY LIGHTING POLE HARDWARE" - LATEST EDITION (ARTBA TECHNICAL BULLETIN NO. 270) EXCEPT AS NOTED.

AVOID PLACING POLES BETWEEN 2' AND 12' FROM EDGE OF SHOULDER ON MAIN ROADS HAVING SIDE SLOPES OF 4:1 OR GREATER

L.P.F. = LIGHT POLE FOUNDATION
T.L. = TRAVEL LANE



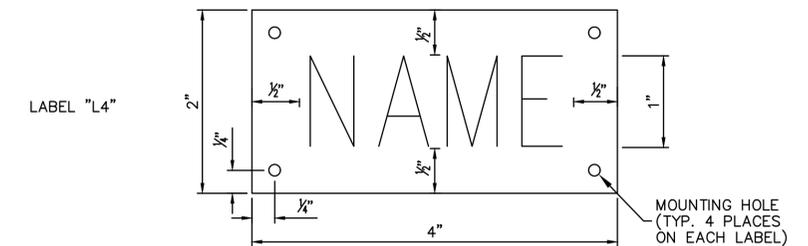
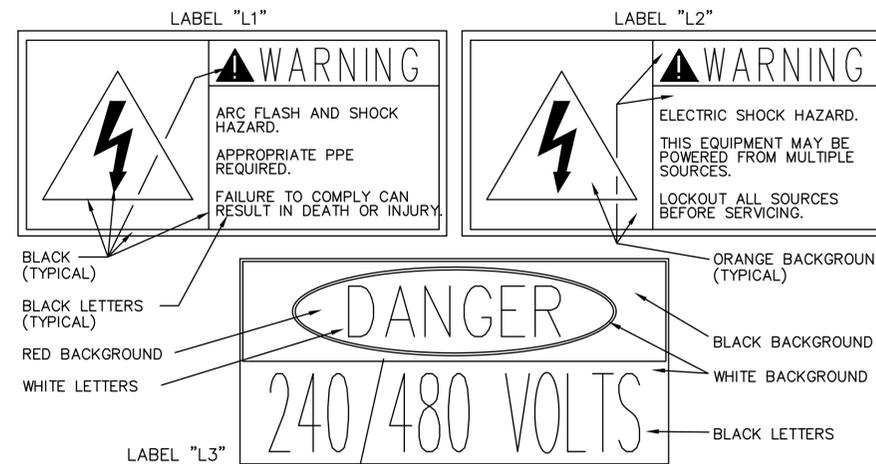
02 LIGHT POLE

N.T.S.

LIGHTING CONTROLLERS, SWITCHBOARDS, CONTROL DESKS, DISCONNECTS, JUNCTION BOXES, GATE HOUSINGS, OTHER ENCLOSURES CONTAINING MOVABLE CONTACTS OR WIRE SIZE(S) #2 COPPER OR LARGER, AND WHERE SPECIFIED BY THE PROJECT ENGINEER SHALL BE FIELD MARKED ACCORDING TO NATIONAL ELECTRICAL CODE (N.E.C.) TO WARN QUALIFIED PERSONNEL OF THE POTENTIAL ELECTRICAL ARC FLASH HAZARDS AND DANGER.

LABEL(S) SHALL BE 5"x7" (MINIMUM). LABEL(S) SHALL BE MADE OF ENGRAVED STOCK WITH MARINE GRADE STAINLESS STEEL FASTENERS HAVING A MINIMUM 30,000 PSI YIELD STRENGTH. LABEL(S) SHALL BE OUTDOOR RATED AND PROTECTED FROM U.V. RADIATION, MOISTURE, OXIDATION, AND OTHER POLLUTANTS. LABEL(S) SHALL BE SURFACE MOUNT AND SUITABLE FOR INSTALLING ON FLAT SURFACES OF METAL, FIBERGLASS, OR PAINT. AFFIX LABEL(S) TO ALL DOORS OF LIGHTING CONTROLLERS, CONTROL CABINETS, CONSOLES, JUNCTION BOXES CONTAINING TERMINAL BLOCKS, GATE HOUSINGS, ETC., AND WHERE SPECIFIED BY THE PROJECT ENGINEER. LABELS SHALL COMPLY WITH MINIMUM REQUIREMENTS SET FORTH BY OSHA 29 CFR PART 1910, NFPA 70, AND NFPA 70E. ARC FLASH PROTECTION (SEE N.E.C. 110.16).

WHERE SHOWN ON THE PLANS OR REQUIRED BY THE PROJECT ENGINEER, ALL LIGHTING CONTROLLERS, SWITCHBOARDS, CONTROL DESKS, DISCONNECTS, JUNCTION BOXES, GATE HOUSINGS, AND OTHER EQUIPMENT ENCLOSURES SHALL HAVE LABEL AS SHOWN BELOW. REFER TO PLAN SHEETS AND DETAILS FOR LABEL NAME AND LOCATION OF LABEL. LABEL SHALL BE FABRICATED FROM 1/8" THICK (MINIMUM) PHENOL PLATE ENGRAVED STOCK. LABEL SHALL HAVE SATIN BLACK OUTER LAYER. LABEL SHALL HAVE WHITE INNER LAYER. LABEL SHALL HAVE 45° BEVELED EDGES. LABEL NAME SHALL HAVE 1" SIZE (MINIMUM) BLOCK-STYLE LETTERS. LABEL SHALL HAVE FOUR (4) 1/8" (MINIMUM) DIAMETER PRE-DRILLED HOLES, ONE (1) LOCATED AT EACH CORNER, FOR MOUNTING LABEL. LABEL SHALL BE FASTENED TO EQUIPMENT USING #6-32 (MINIMUM) MARINE DUTY STAINLESS STEEL SELF-TAPPING MACHINE SCREWS HAVING 30000 PSI (MINIMUM) YIELD STRENGTH. LABEL SHALL BE LEVEL AFTER INSTALLATION. INSTALL LABELS WHERE APPLICABLE OR AS INDICATED IN THE PLANS.



01 ELECTRICAL LABELS

N.T.S.

03 ELECTRICAL LABELS

N.T.S.

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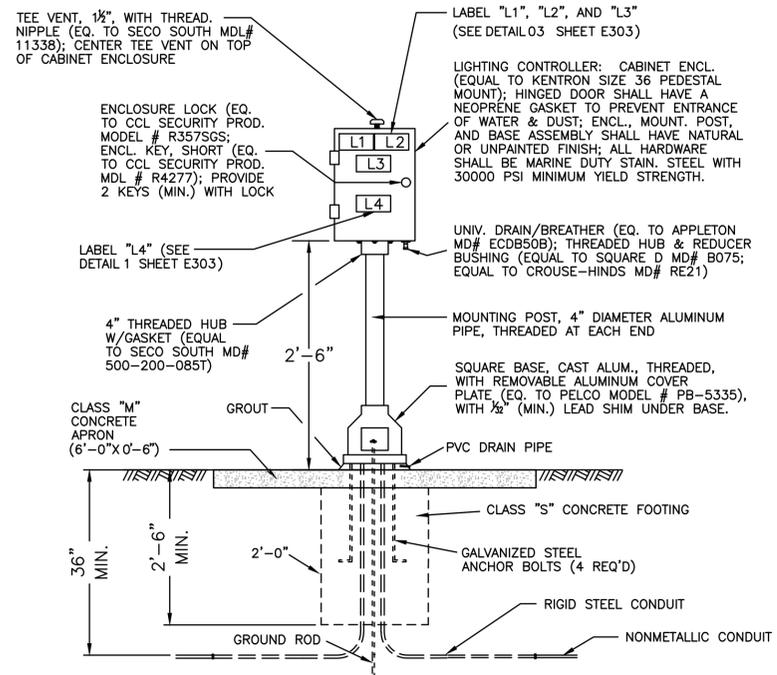
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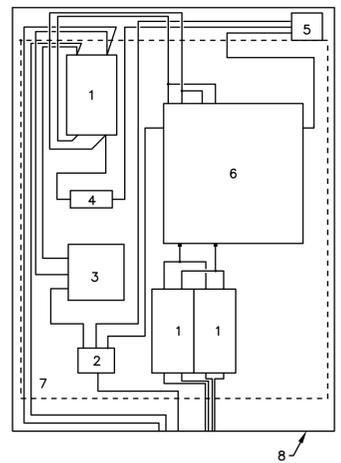
SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
CADDO PARISH, LOUISIANA

PROJECT NO.: 47786.038
ISSUED: 02/06/2026
SCALE: AS NOTED
SHEET TITLE
ELECTRICAL DETAILS
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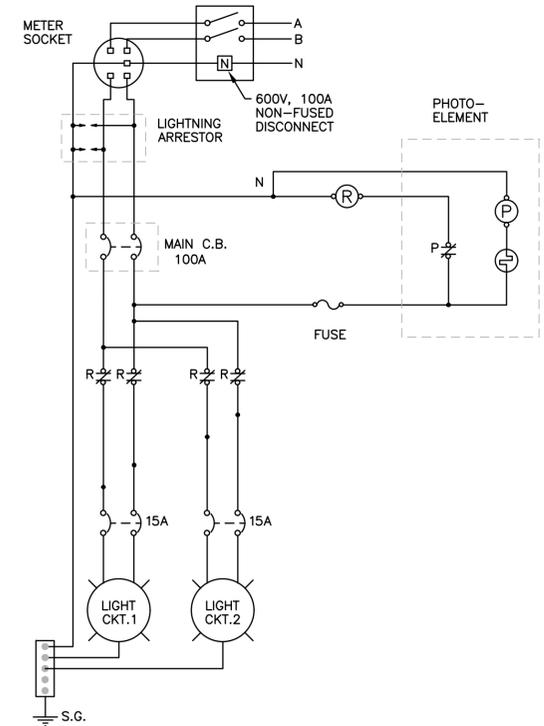
04 PEDESTAL LIGHTING CONTROLLER
N.T.S.



- CIRCUIT BREAKERS:** 600 VAC RATED, 10kAIC (MIN.) @ 480 VAC, 2 POLE, 15A TRIP BRANCH CIRCUIT BREAKERS, 2P-100A MAIN.
- GROUNDING TERMINAL:** PWR. DIST. BLOCK, INTERMEDIATE, BOX TO BOX, CU STUD, 600 V RATED, W/ SAFETY COVERS (EQ. TO FERRAZ-SHAWMUT MD#66512-08570); MOUNT WITH STAINLESS STEEL HARDWARE.
- SURGE ARRESTOR:** 600 VAC RATED PHASE TO GND. (MAX.), 50/60 HZ, 40kA (MAX.) SURGE AMPS, THERMAL FUSING, U.L. LISTED (EQUAL TO SQUARE D MODEL #SPDN-D240-2S)
- CONTROL CIRCUIT FUSE & FUSEHOLDER:** FUSE: CLASS CC, FAST-ACTING, 600 VAC, 7 AMP, U.L. LISTED (EQUAL TO COOPER-BUSSMANN MD#TK-R-7); FUSEHOLDER: CLASS CC, 30 A, 600 VAC, U.L. LISTED, 1 POLE, COPPER BOX LUG TERMINALS (EQUAL TO COOPER-BUSSMANN MD# BC6031B)
- PHOTO-ELECTRIC CONTROL:** TWIST LOCK W/ RECEPTACLE, MOUNTING BRKT., SOLID STATE, HERMETICALLY SEALED, 105-300 VAC, 1800 VA, TIME DELAY SWITCHING, N.C. CONTACT, OPER. SWITCH LEVELS 2.0 F.C. ON/OFF + 20%, TEMP. RANGE -40° F TO +140° F, DRILL 1/4" DIA. HOLE IN CABINET, CEMENT PLEXIGLASS OVER HOLE, INSTALL ASSEMBLY IN CABINET W/PHOTOCELL CONTROL WINDOW 1/4" FROM HOLE, POSITION PHOTOCELL CONTROL WINDOW EYE TO THE NORTH (EQ. TO PRECISION MD #ECDV-AP-TD)
- LIGHTING CONTACTOR:** ELECTRIC, HELD, N.O. CONTACTS, 600 VAC RATED, 480 VAC COIL, HIGH PRESSURE BOX LUG TERMINALS; NUMBER & CURRENT RATING OF CONTACTS AS SHOWN ON CKT. SCHED. (EQ. TO SQUARE D CLASS 8903)
- MOUNTING PLATE:** 1/2" INSULATED MTG. BOARD, PHENOLIC LAMINATE, NEMA GRADE X (EQUAL TO PORT PLASTICS MD# NP610), TAN NATURAL COLOR, MOUNT TO BOSS FEET ON BOX WITH STAINLESS STEEL FLAT HEAD SCREWS, COUNTER-SINK HOLES TO MATCH SCREWS
- CABINET:** CAST ALUMINUM OR STAINLESS STEEL W/ PEDESTAL ADAPTER, GASKETED DOOR W/ LOCK & KEY, TEE VENT, 24"W X 35"H X 14"D. (EQ. TO KENTRON SIZE 36 PEDESTAL MOUNT)

NOTE:
1. STAIN. STEEL HARDWARE SHALL BE MARINE GRADE W/ 30,000 PSI (MIN.) YIELD STRENGTH.
2. ALLOW 2" (MIN.) CLEARANCE BTWN. COMPONENTS; 4" (MIN.) CLEARANCE ON ALL SIDES.
3. REFER TO DETAIL PEDESTAL LIGHTING CONTROLLER FOR ADDITIONAL SPECIFICATIONS.

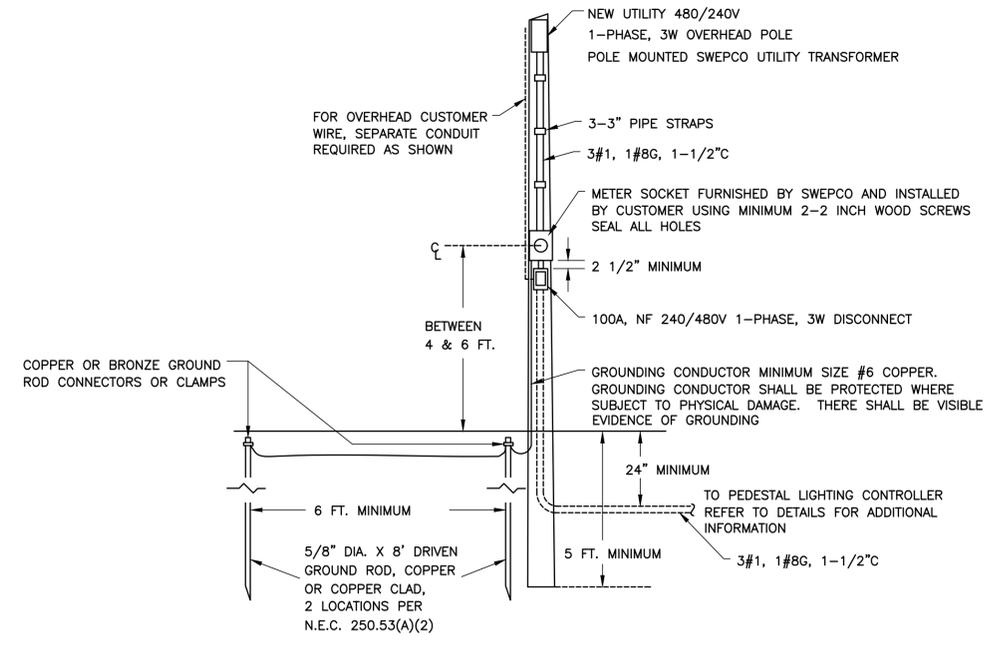
03 LIGHTING CONTROLLER LAYOUT
N.T.S.



- NOTES:
- SERVICE: 240/480 VOLT, GROUND, 1-PHASE, 60 HZ.
 - WIRING SCHEMATIC SHOWN IS A GENERAL WIRING LAYOUT. NUMBER OF CIRCUIT BREAKERS AND RELAY CONTACTS MAY VARY. (SEE DETAIL LIGHTING CONTROLLER LAYOUT AND CIRCUIT SCHEDULE)
 - CONTRACTOR SHALL OBTAIN APPROVAL FROM UTILITY CO. BEFORE INSTALLATION.
 - S.G. = SERVICE GROUND.
 - METER SOCKET TO BE FURNISHED AND INSTALLED BY CONTRACTOR AS PER UTIL. CO. REQUIREMENTS; METER TO BE FURNISHED AND INSTALLED BY UTILITY COMPANY.

* SCHEMATIC SHOWN AT NIGHT WITH LIGHTS ON.

02 LIGHTING CONTROL SCHEMATIC
N.T.S.



01 ELECTRICAL RISER
N.T.S.

NO.	DATE	DESCRIPTION

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SOUTHFIELD ROAD IMPROVEMENTS
CITY OF SHREVEPORT
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E304